



Nashville Area Metropolitan Planning Organization Tri-County Transportation & Land Use Study of Robertson, Sumner, Wilson Counties

Community at Large Sessions: The Importance of Quality Growth
October 6, 7, 9, 2008



LandDesign • Kimley-Horn & Associates, Inc.
Basile Baumann Prost Cole & Associates, Inc. • Sterling Communications



The Project Team

Nashville Area Metropolitan Planning Organization

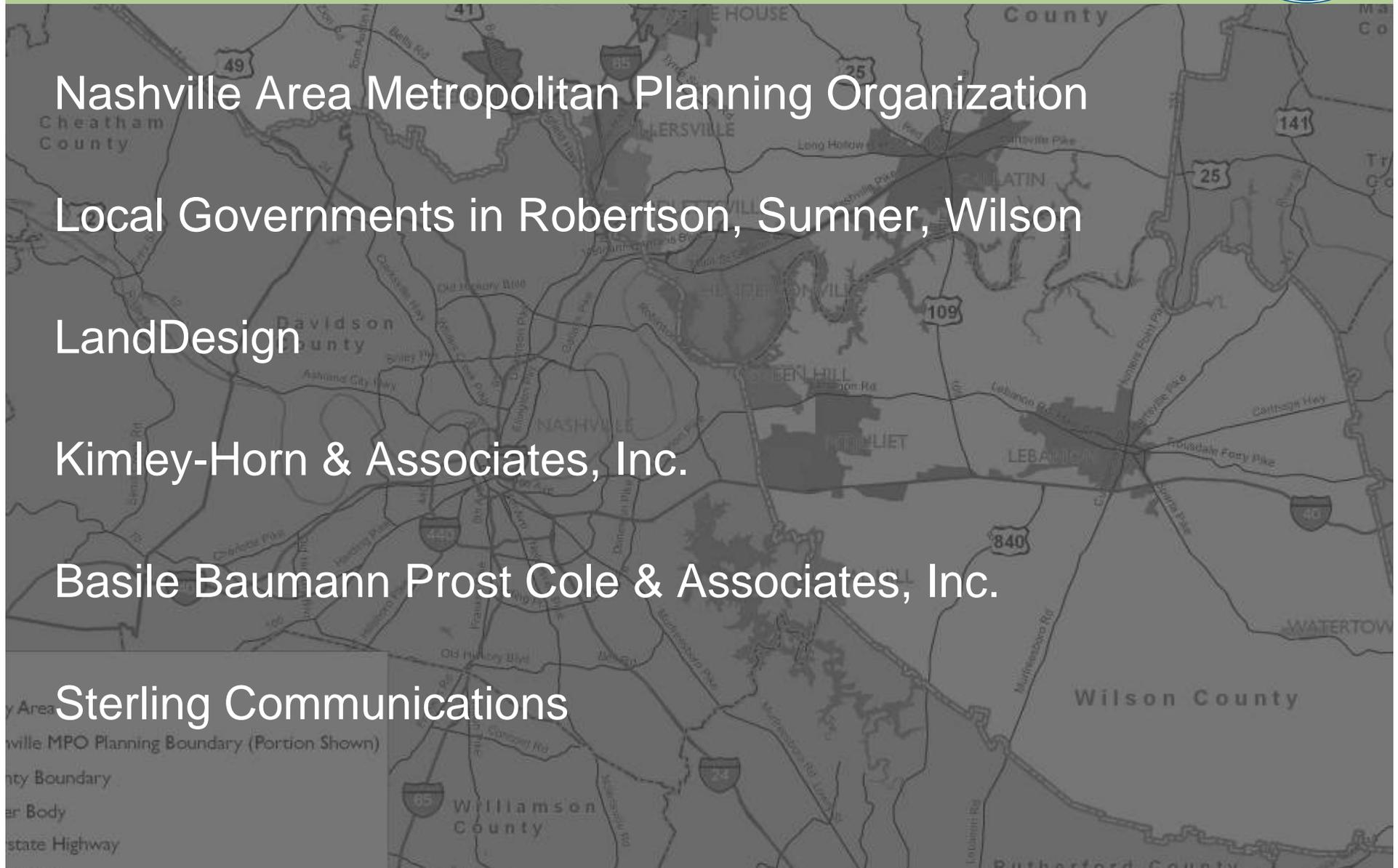
Local Governments in Robertson, Sumner, Wilson

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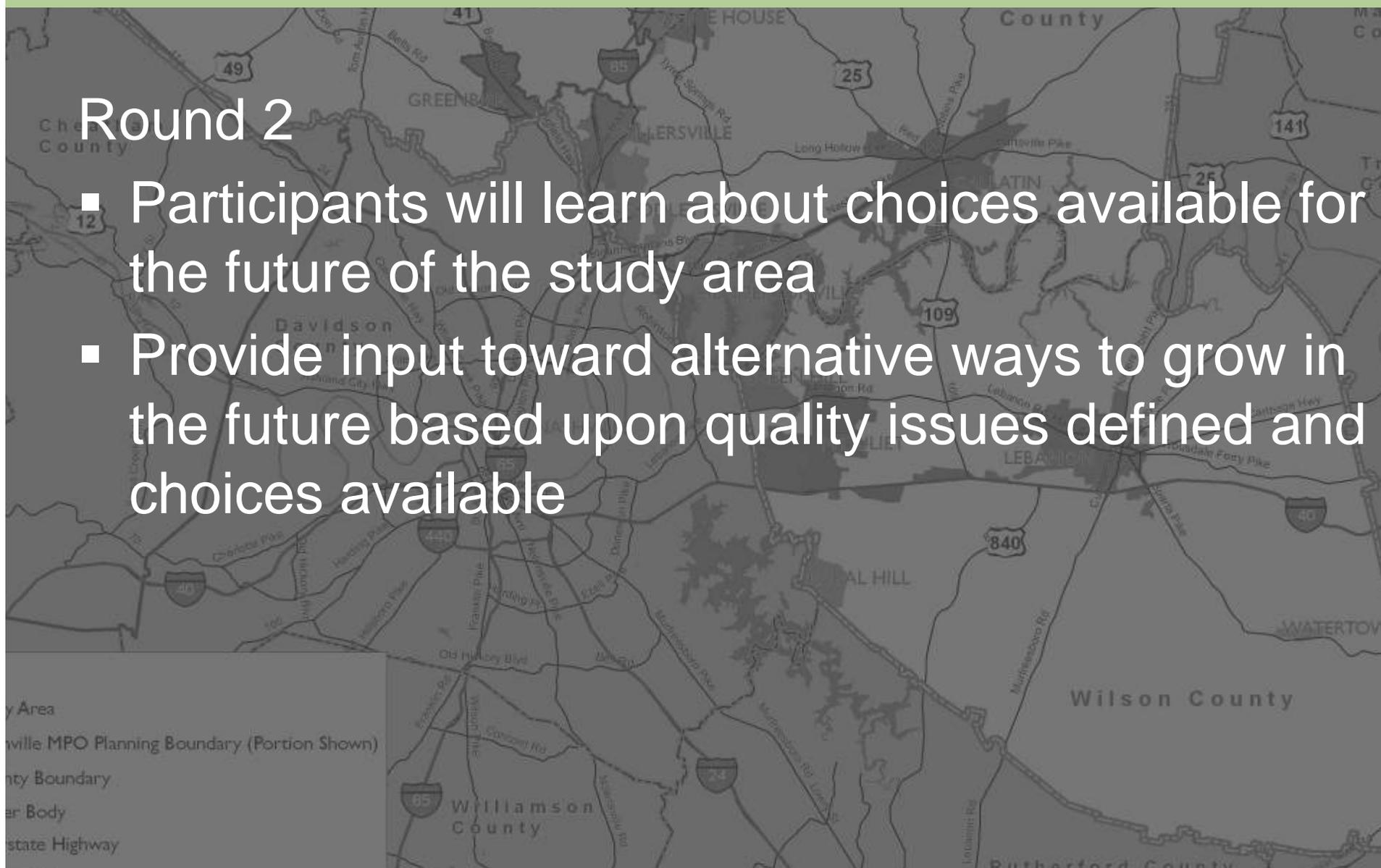
Purpose – Why are we here?

- To bring local governments, citizens, and businesses together to talk about growth issues
- To create a forum for local leaders to consider growth plans of their neighboring communities for regional mobility and prosperity
- To generate ideas for the 2035 Regional Transportation Plan, scheduled for adoption in Fall 2009

Objectives / Outcomes of Session

Round 2

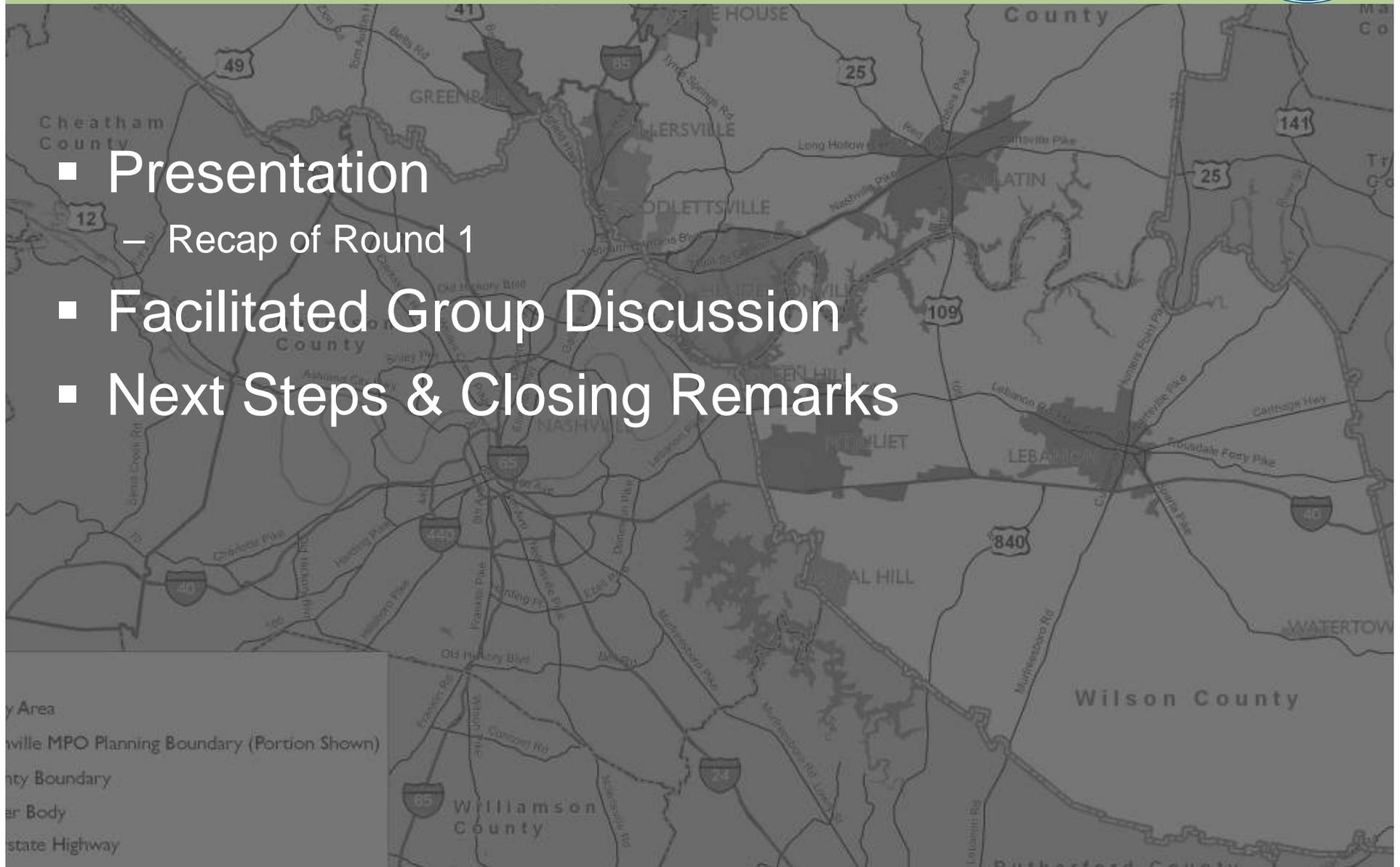
- Participants will learn about choices available for the future of the study area
- Provide input toward alternative ways to grow in the future based upon quality issues defined and choices available



Agenda

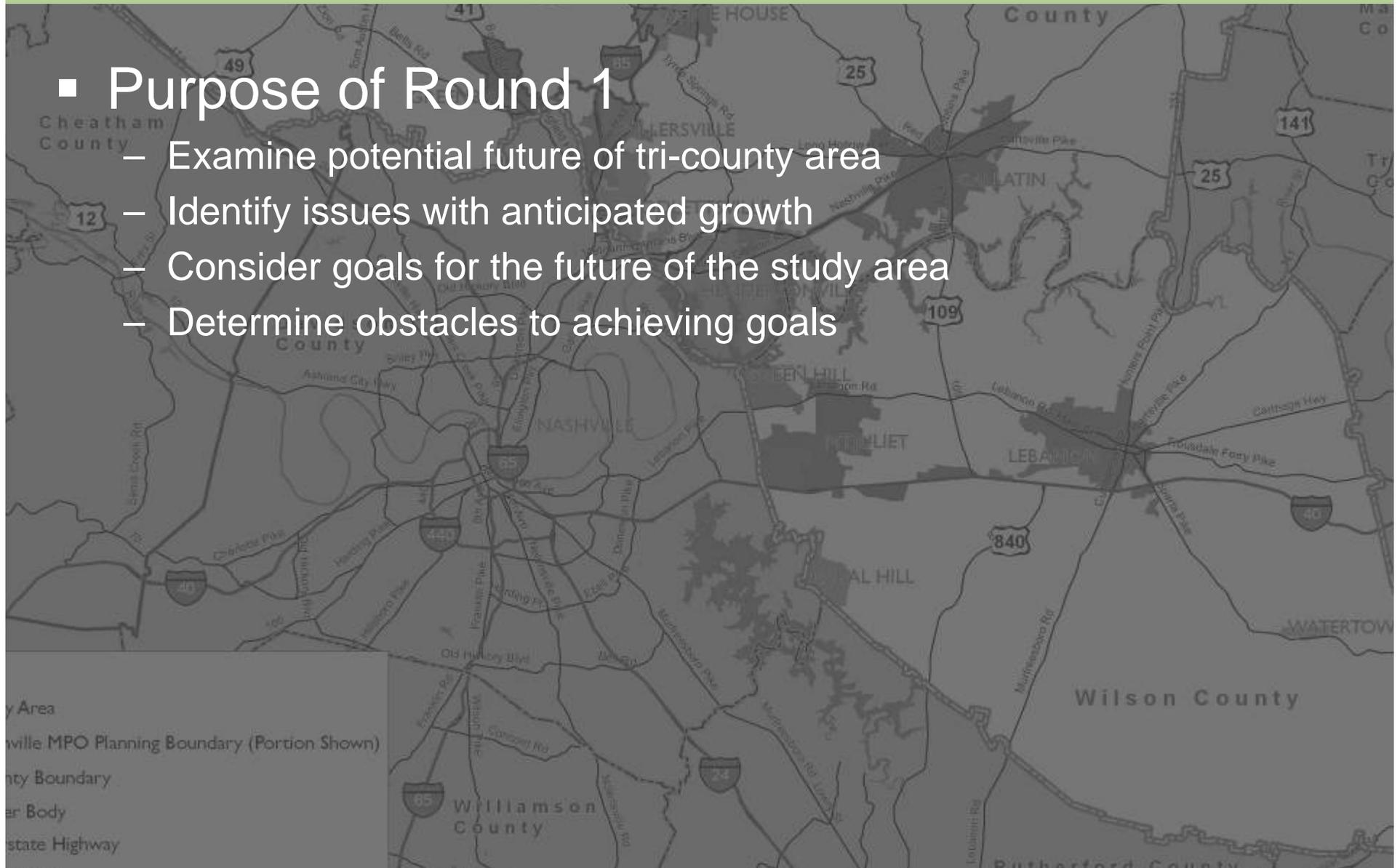


- Presentation
 - Recap of Round 1
- Facilitated Group Discussion
- Next Steps & Closing Remarks

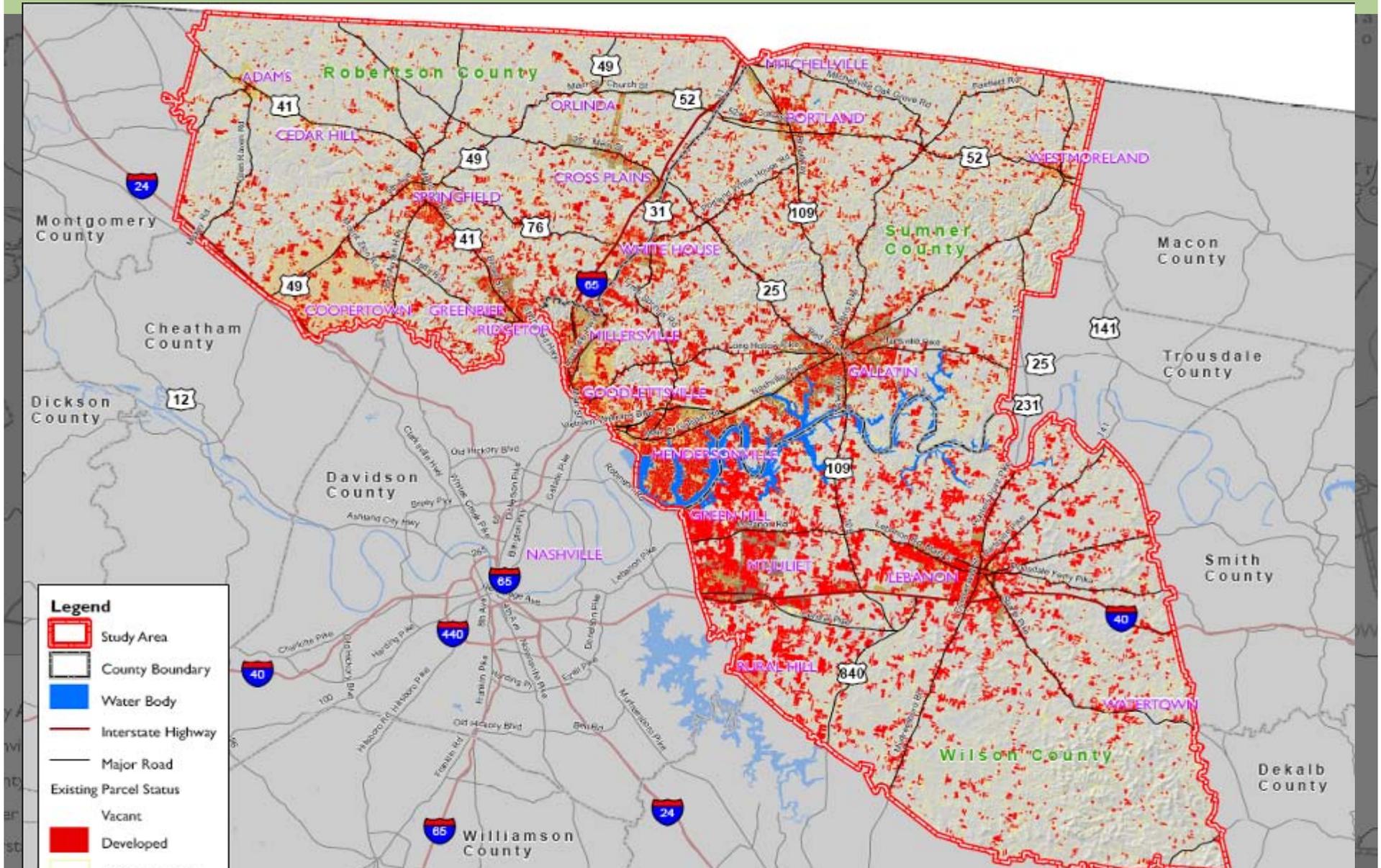


Recap of Round 1

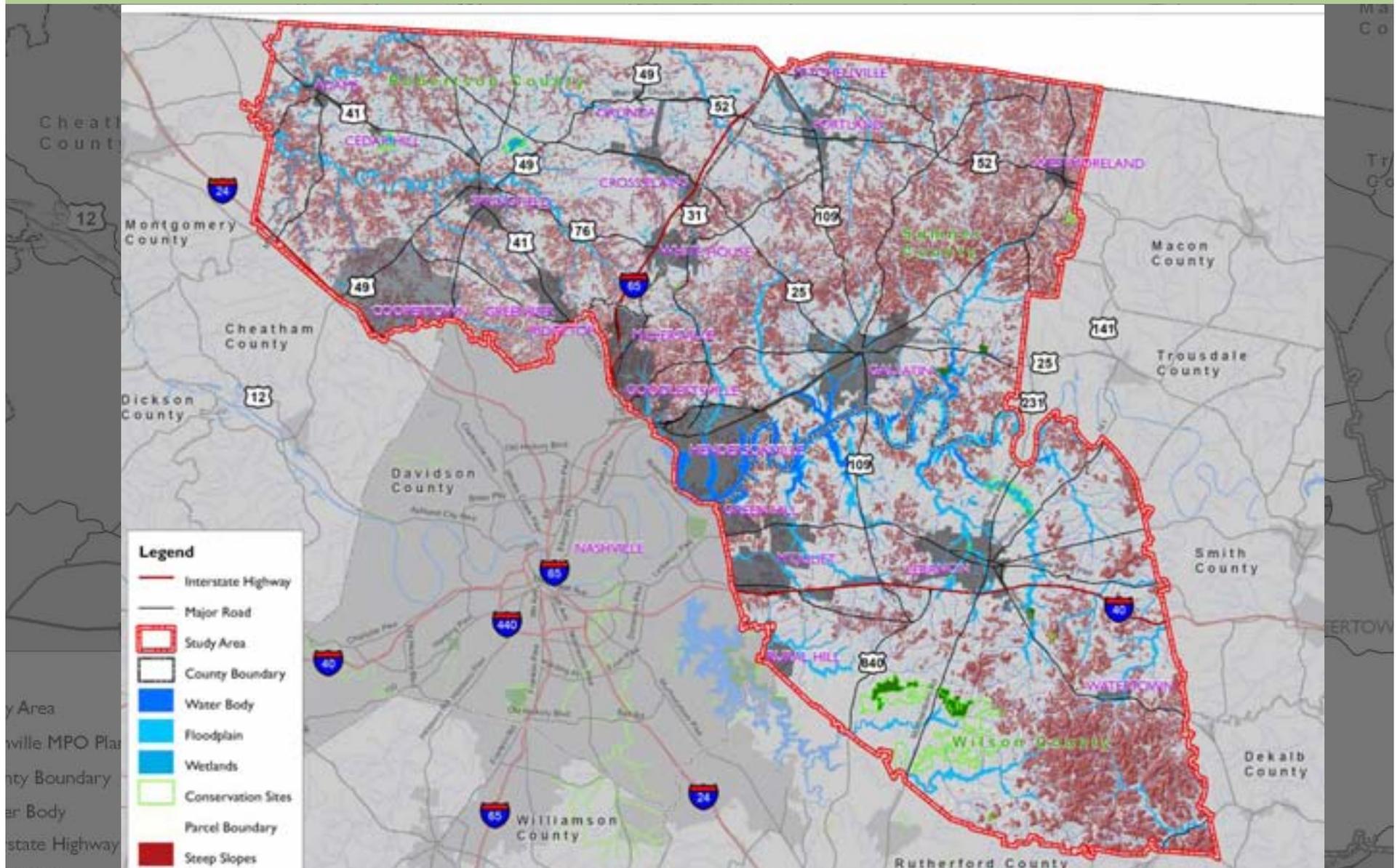
- Purpose of Round 1
 - Examine potential future of tri-county area
 - Identify issues with anticipated growth
 - Consider goals for the future of the study area
 - Determine obstacles to achieving goals



Supply - Existing Development

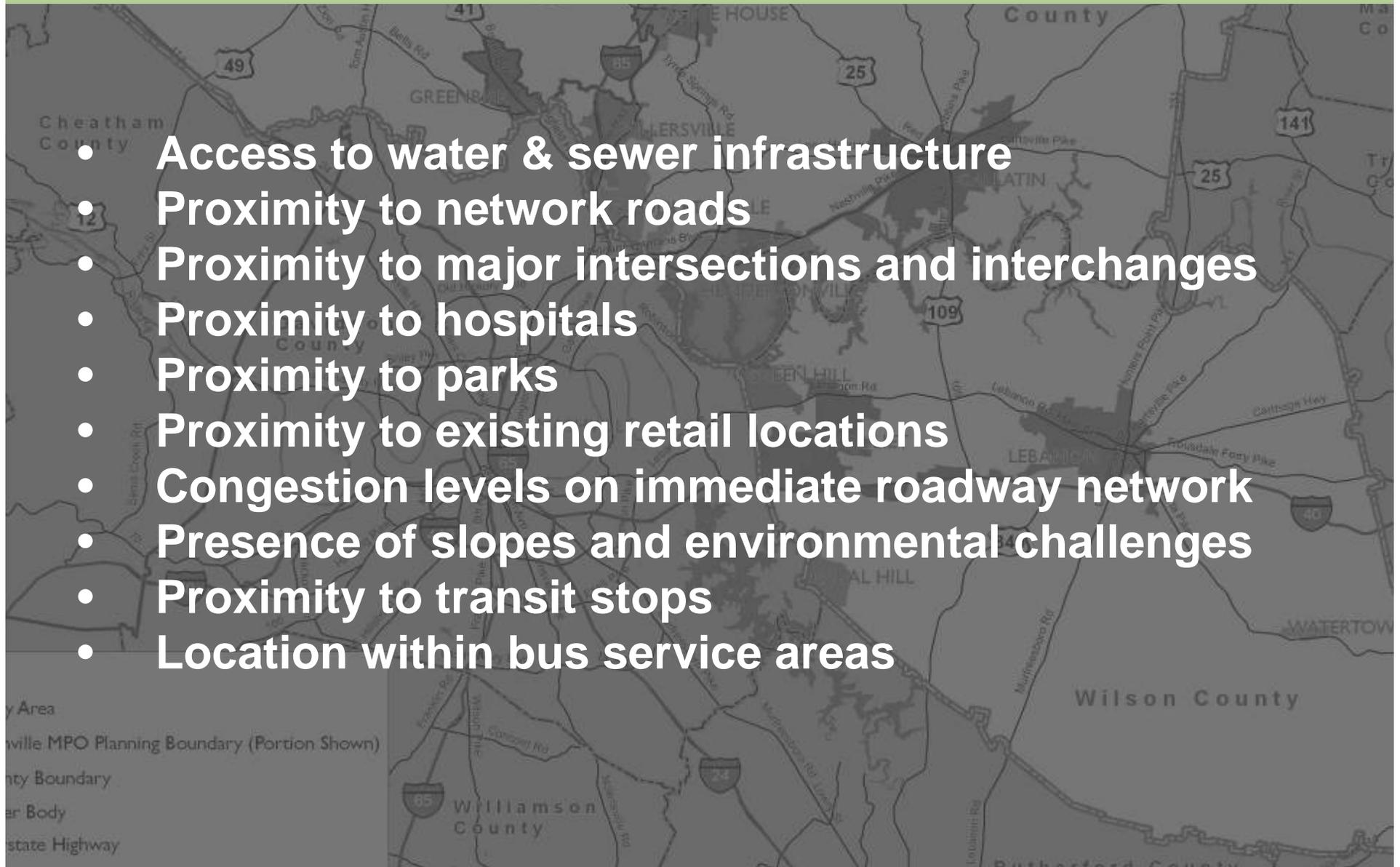


Supply – Environmental Constraints (Environment, Parks, and Open Space)

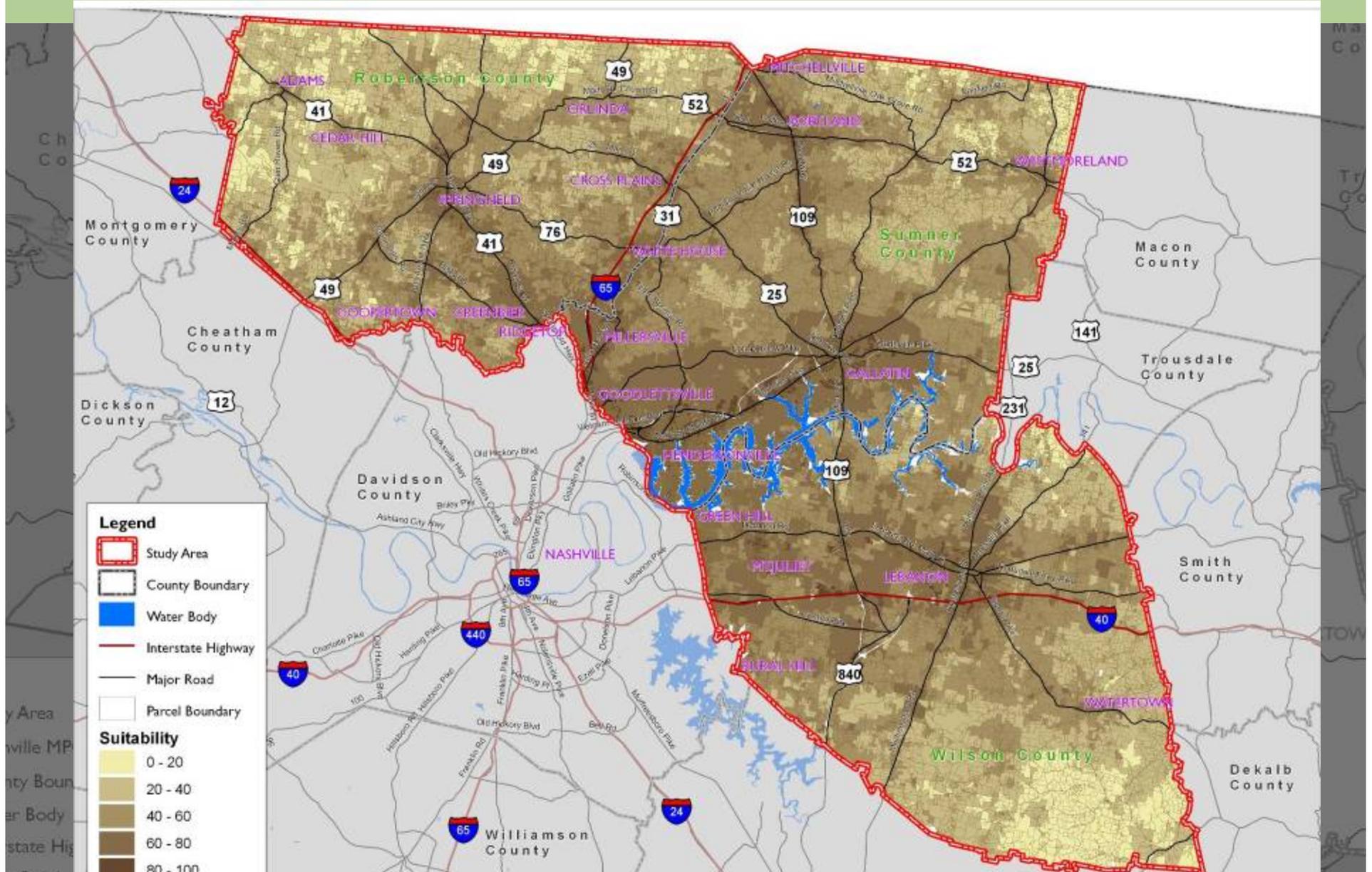


Suitability Factors

- Access to water & sewer infrastructure
- Proximity to network roads
- Proximity to major intersections and interchanges
- Proximity to hospitals
- Proximity to parks
- Proximity to existing retail locations
- Congestion levels on immediate roadway network
- Presence of slopes and environmental challenges
- Proximity to transit stops
- Location within bus service areas



Tri-County Suitability Map



Results of Group Exercise – Report Card on BAU

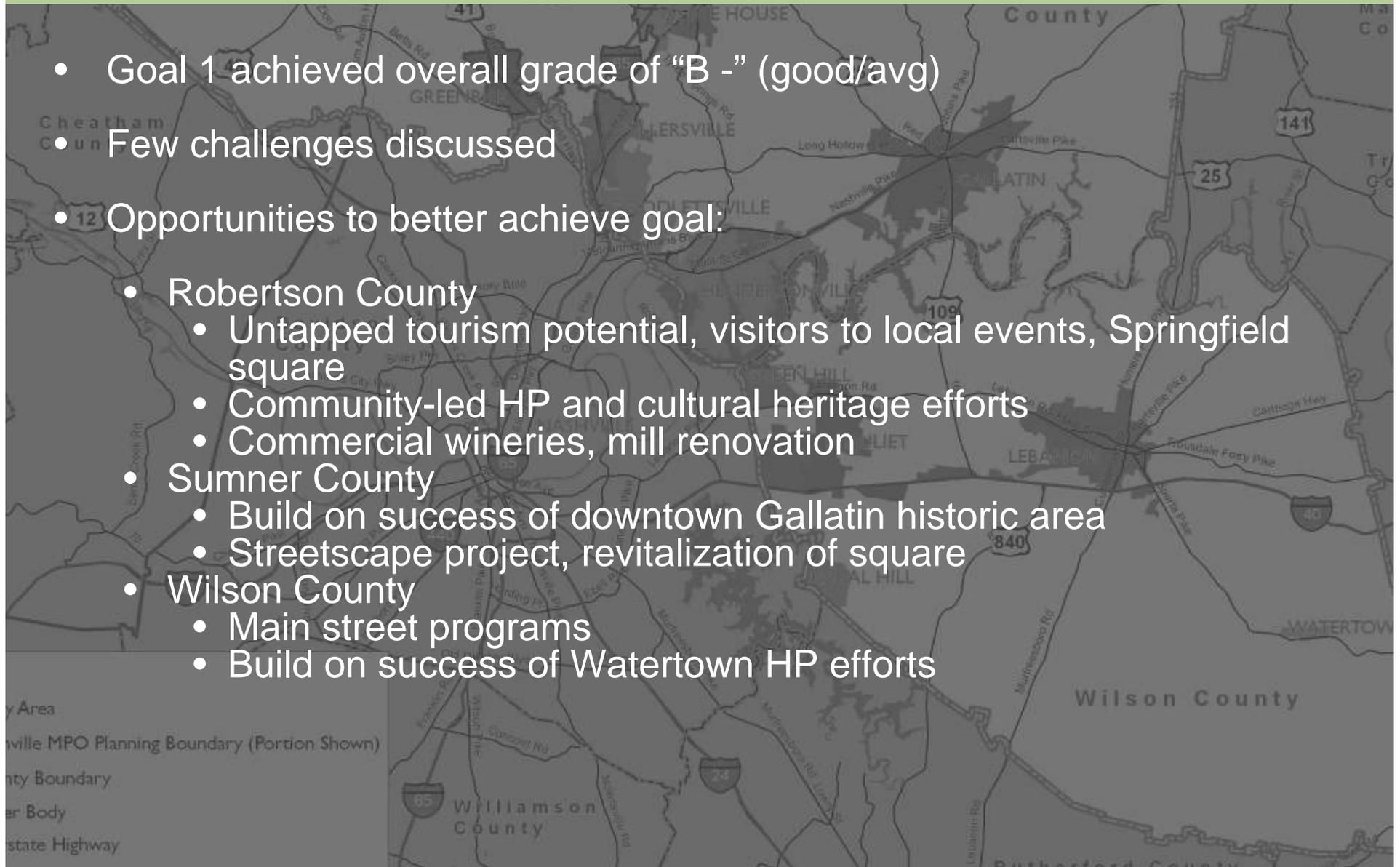
Goals	Grade
Historic Conservation and Enhancement	B-
Viable Agriculture	B
Rural Preservation	C+
Economic Enrichment while Safeguarding Existing Public and Private Development	B-
Preserve Urban Centers	B
Protection of Natural Resources	B-
Efficient Transportation System	C
Ensure Availability of Services	B
Provide Housing Options	C+
Maintain Sense of Community and Sense of Place	B-

Goal 1: Historic Conservation and Enhancement

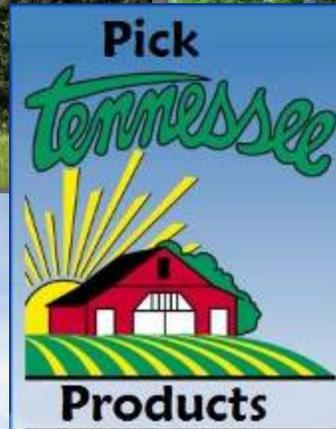


Goal 1: Historic Conservation and Enhancement

- Goal 1 achieved overall grade of “B -” (good/avg)
- Few challenges discussed
- 12 Opportunities to better achieve goal:
 - Robertson County
 - Untapped tourism potential, visitors to local events, Springfield square
 - Community-led HP and cultural heritage efforts
 - Commercial wineries, mill renovation
 - Sumner County
 - Build on success of downtown Gallatin historic area
 - Streetscape project, revitalization of square
 - Wilson County
 - Main street programs
 - Build on success of Watertown HP efforts

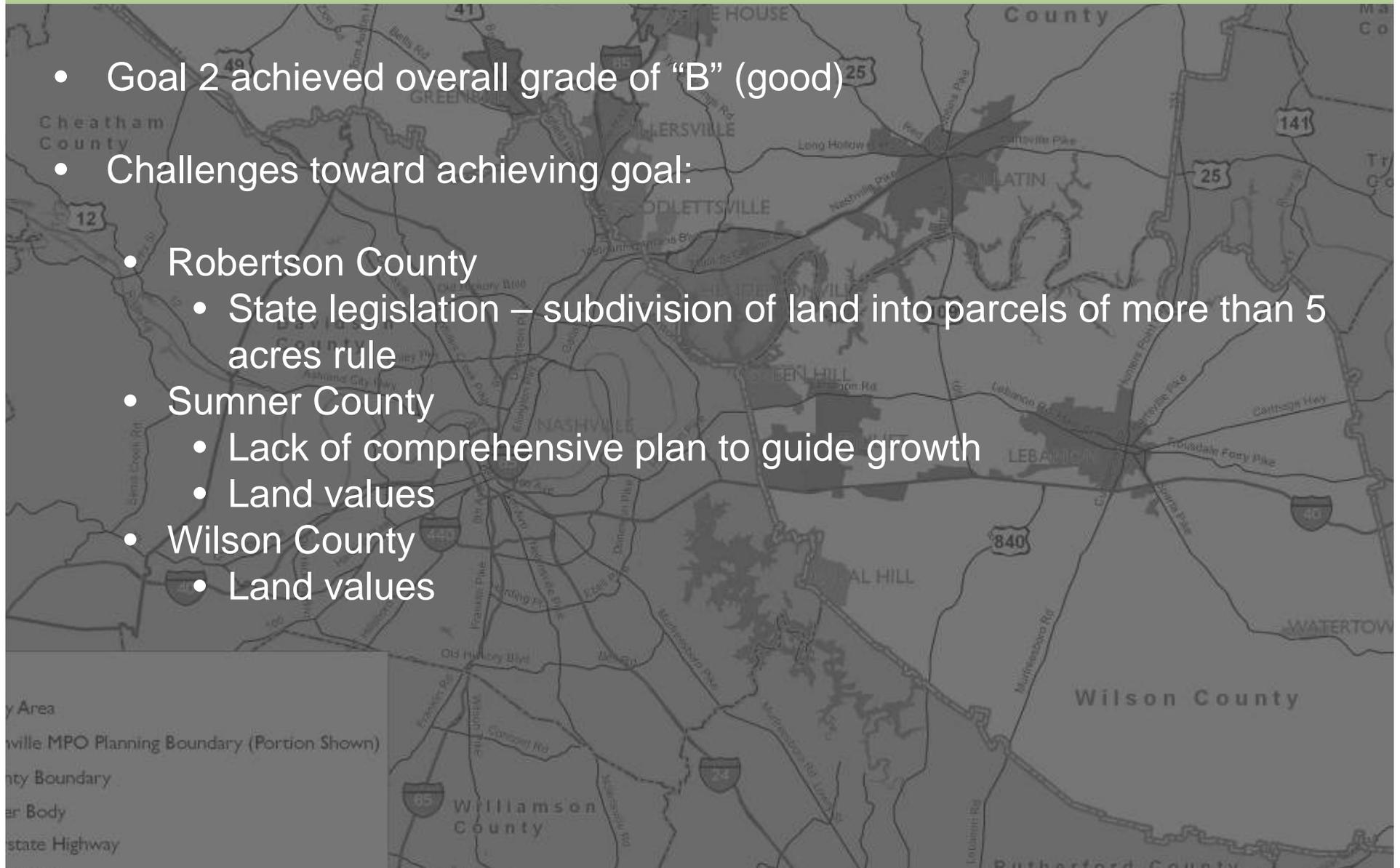


Goal 2: Viable Agriculture

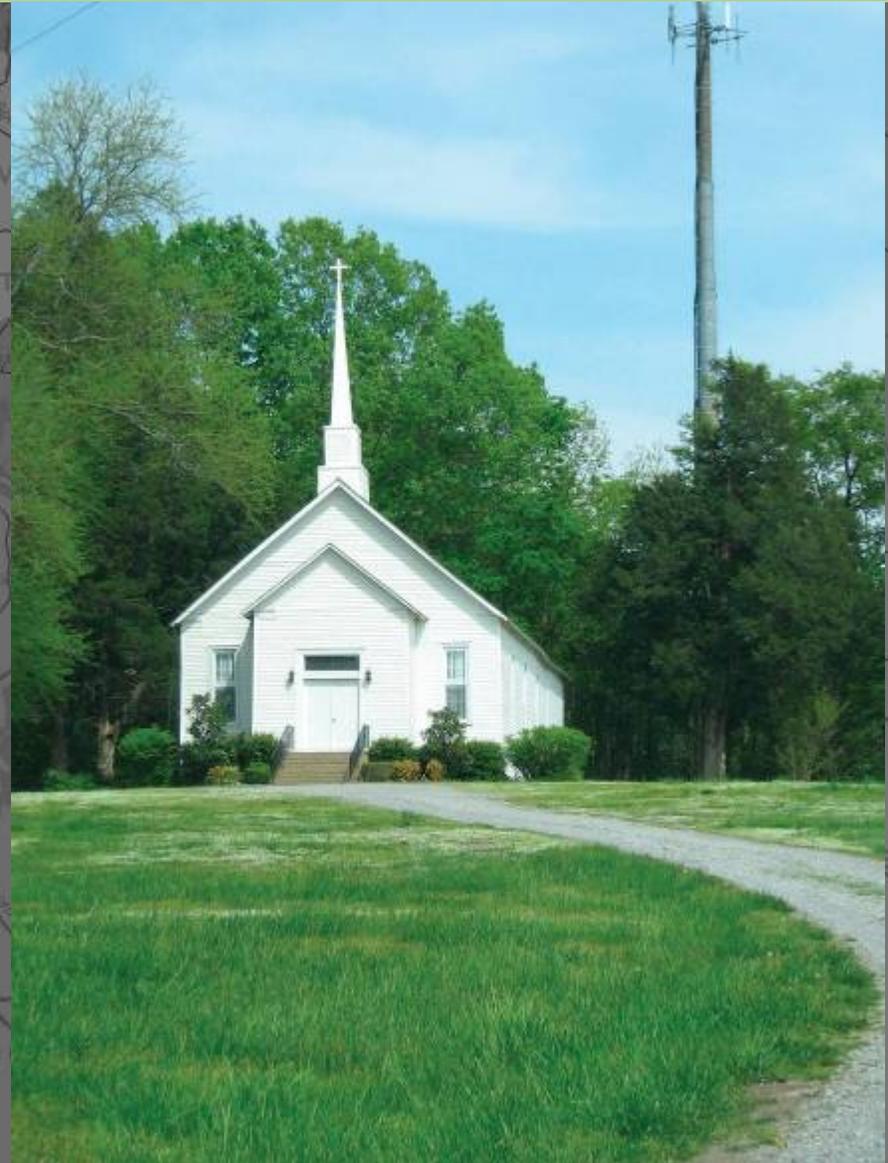
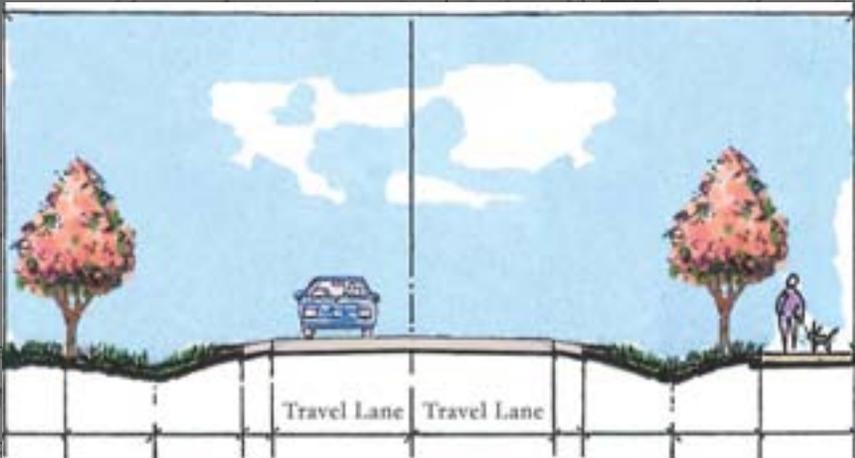


Goal 2: Viable Agriculture

- Goal 2 achieved overall grade of “B” (good)
- Challenges toward achieving goal:
 - Robertson County
 - State legislation – subdivision of land into parcels of more than 5 acres rule
 - Sumner County
 - Lack of comprehensive plan to guide growth
 - Land values
 - Wilson County
 - Land values



Goal 3: Rural Preservation

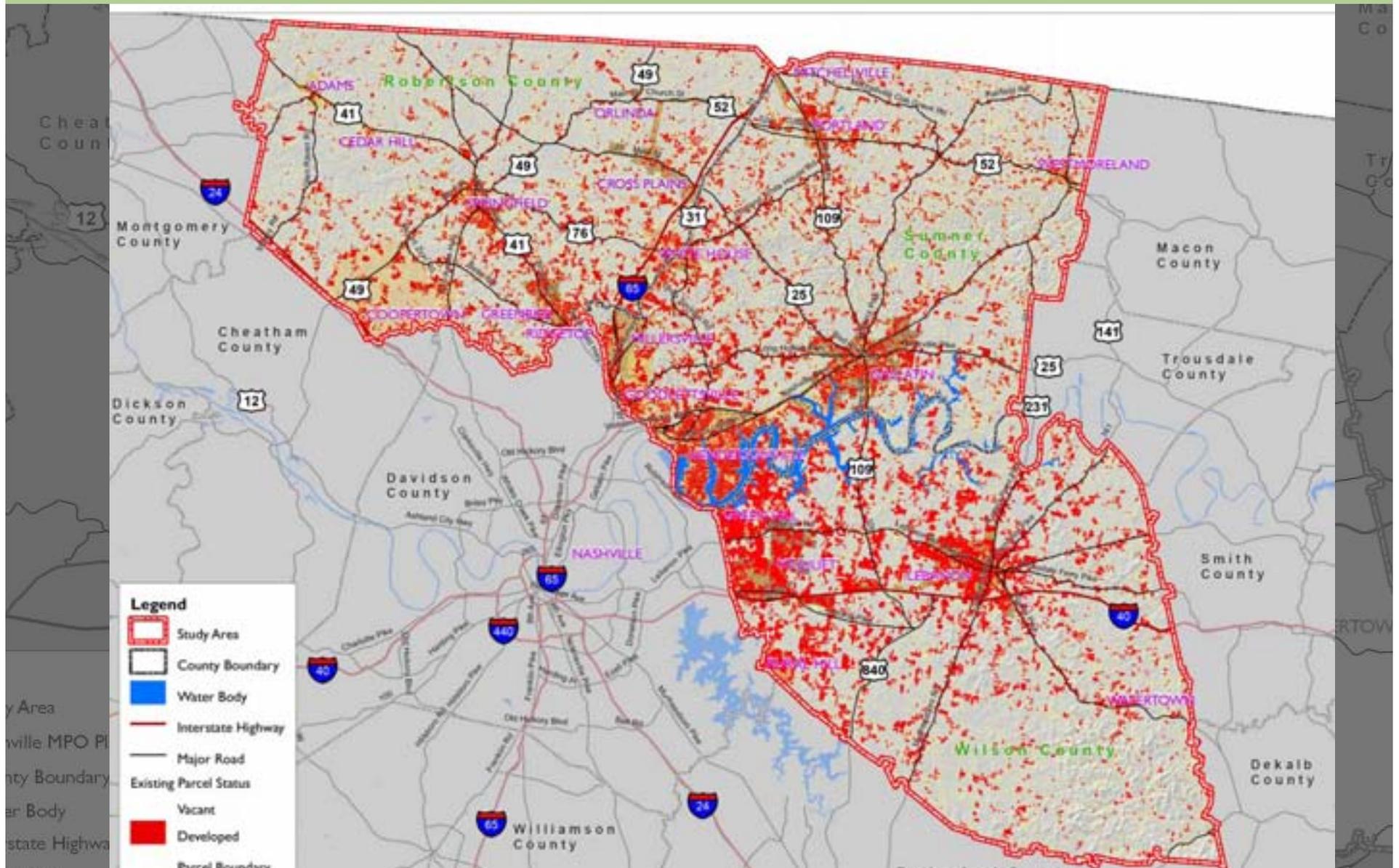


Goal 3: Rural Preservation

Goal 3 achieved overall grade of “C+” (avg)

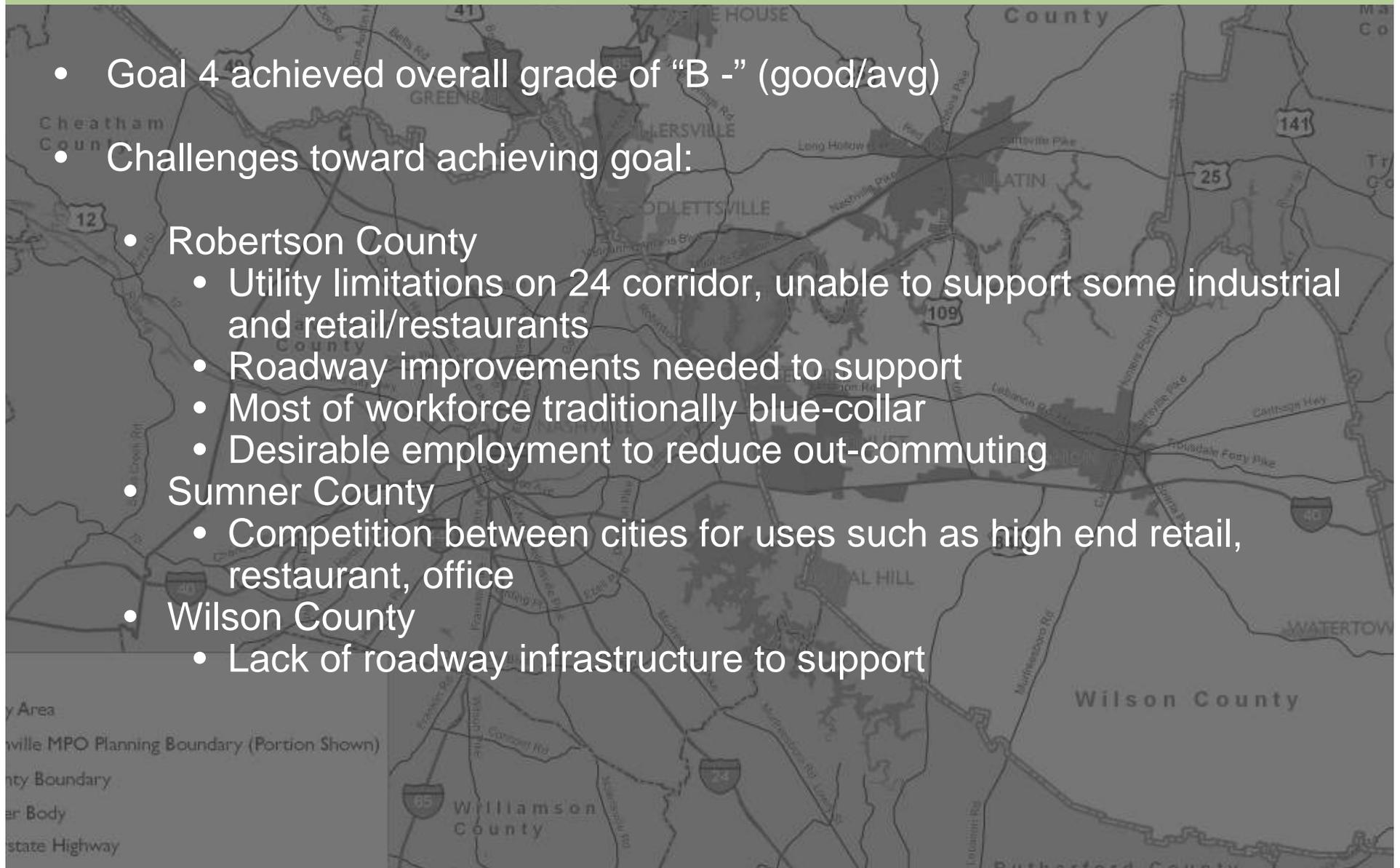
- Viable agriculture and rural have some overlap
- Challenges toward achieving goal:
 - Robertson County
 - State legislation – 5 ac rule and development trends
 - Private property rights
 - Sumner County
 - Lack of comprehensive plan to guide growth
 - Utility systems allow development in most areas of county
 - Land values
 - Wilson County
 - Land values
 - Most of the county designated for low density residential development
 - Utility systems allow development in most areas of county

Goal 4: Economic Enrichment while Safeguarding Existing Public and Private Development

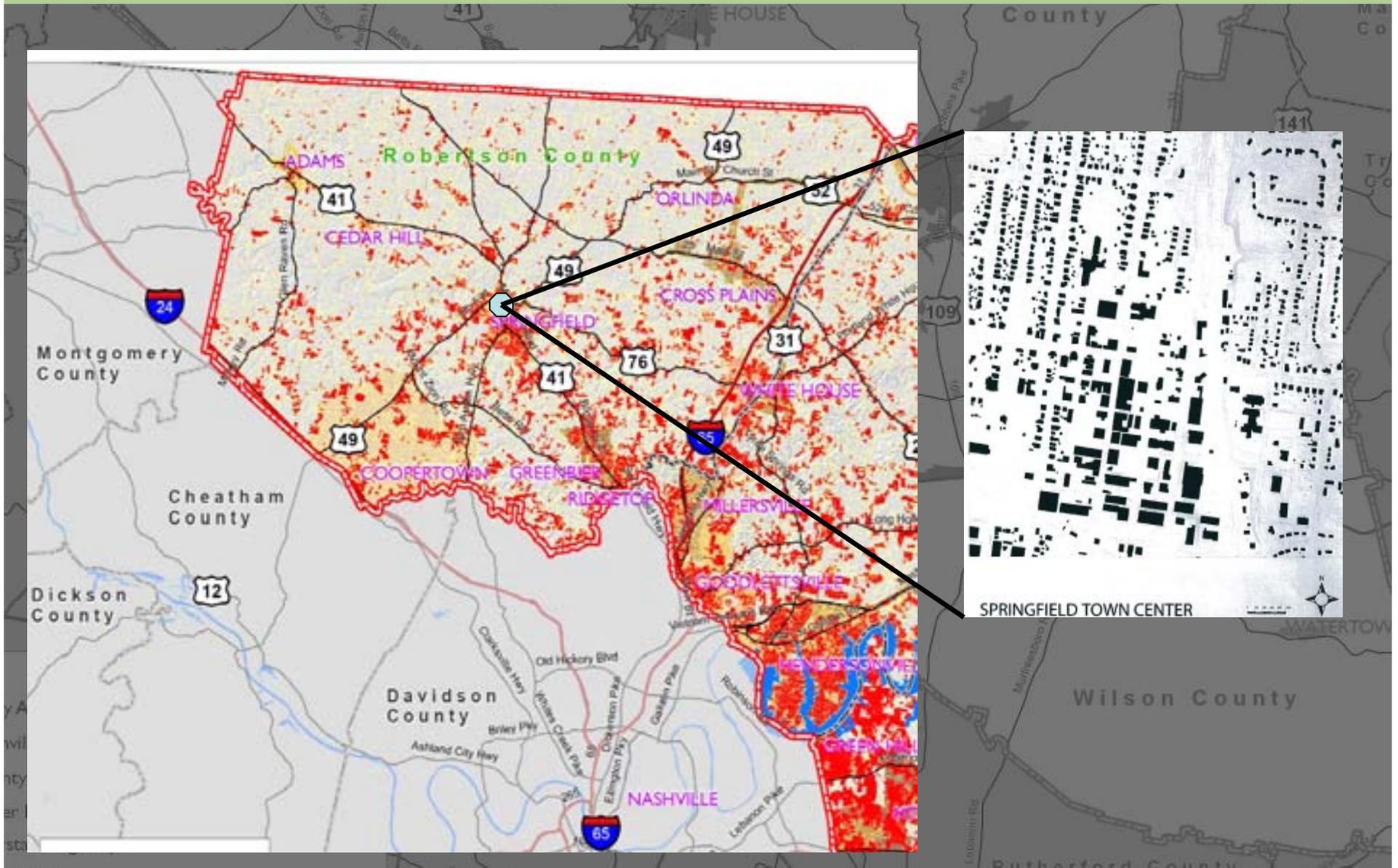


Goal 4: Economic Enrichment while Safeguarding Existing Public and Private Development

- Goal 4 achieved overall grade of “B -” (good/avg)
- Challenges toward achieving goal:
 - Robertson County
 - Utility limitations on 24 corridor, unable to support some industrial and retail/restaurants
 - Roadway improvements needed to support
 - Most of workforce traditionally blue-collar
 - Desirable employment to reduce out-commuting
 - Sumner County
 - Competition between cities for uses such as high end retail, restaurant, office
 - Wilson County
 - Lack of roadway infrastructure to support

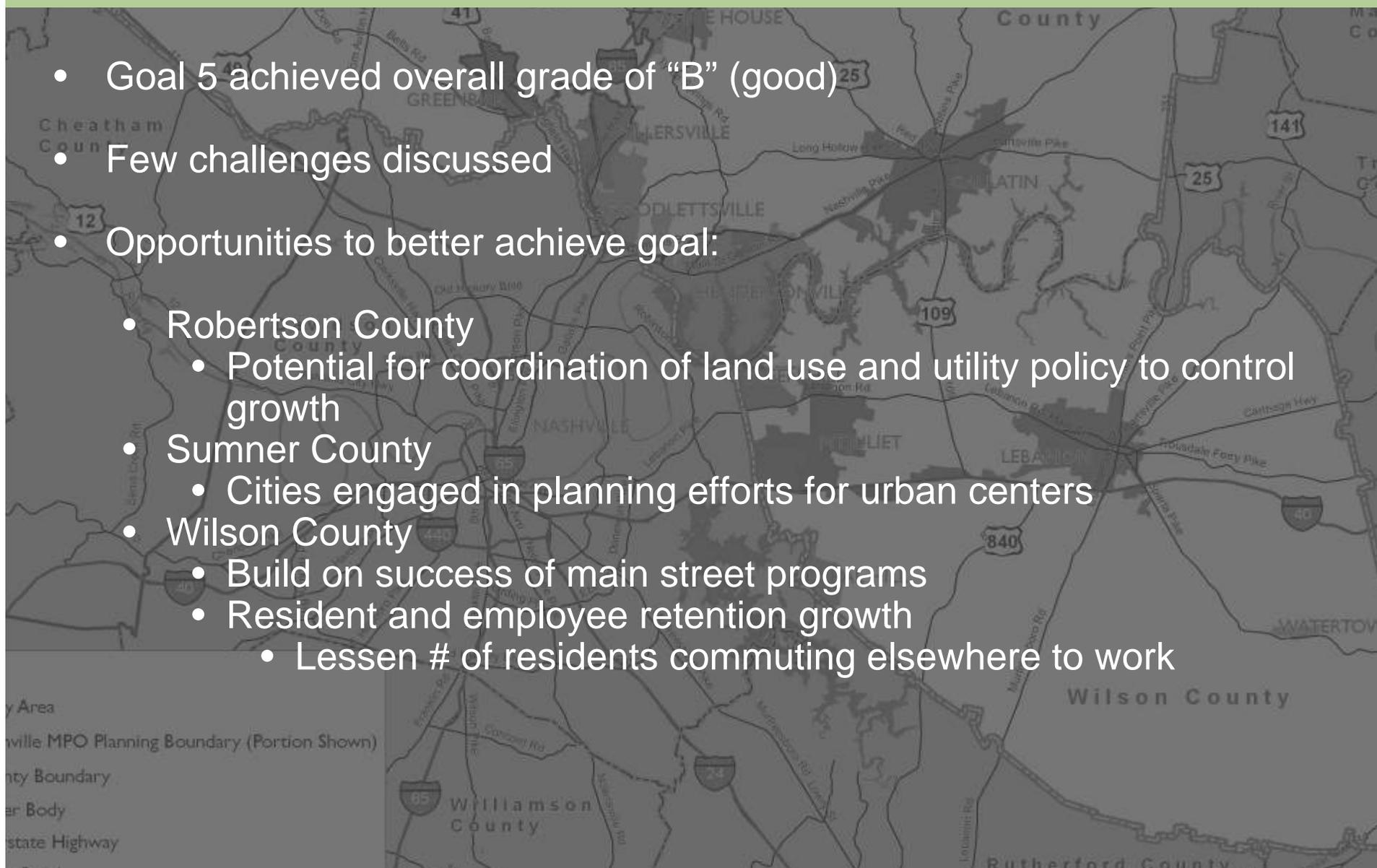


Goal 5: Preserve Urban Centers



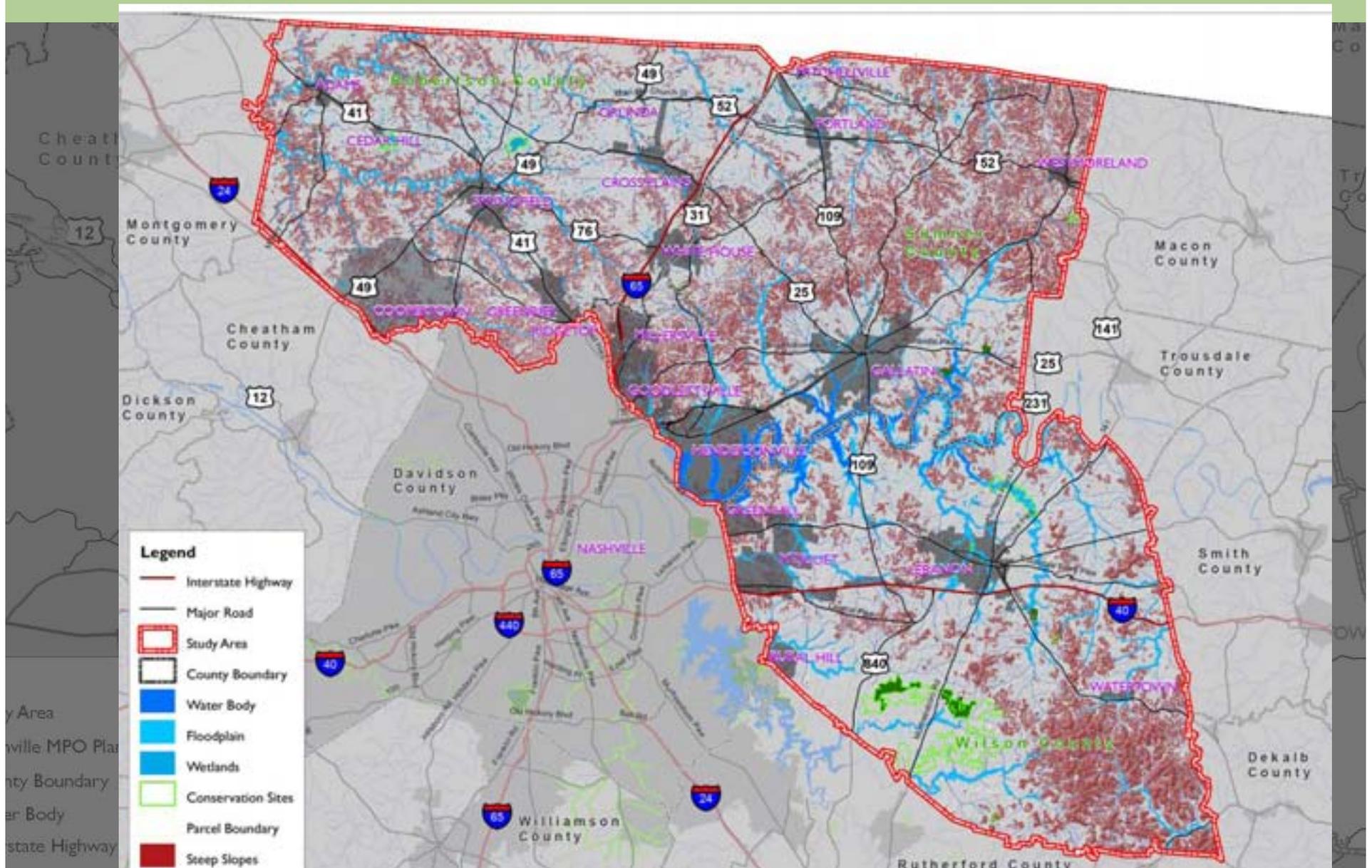
Goal 5: Preserve Urban Centers

- Goal 5 achieved overall grade of “B” (good)
 - Few challenges discussed
- Opportunities to better achieve goal:
 - Robertson County
 - Potential for coordination of land use and utility policy to control growth
 - Sumner County
 - Cities engaged in planning efforts for urban centers
 - Wilson County
 - Build on success of main street programs
 - Resident and employee retention growth
 - Lessen # of residents commuting elsewhere to work



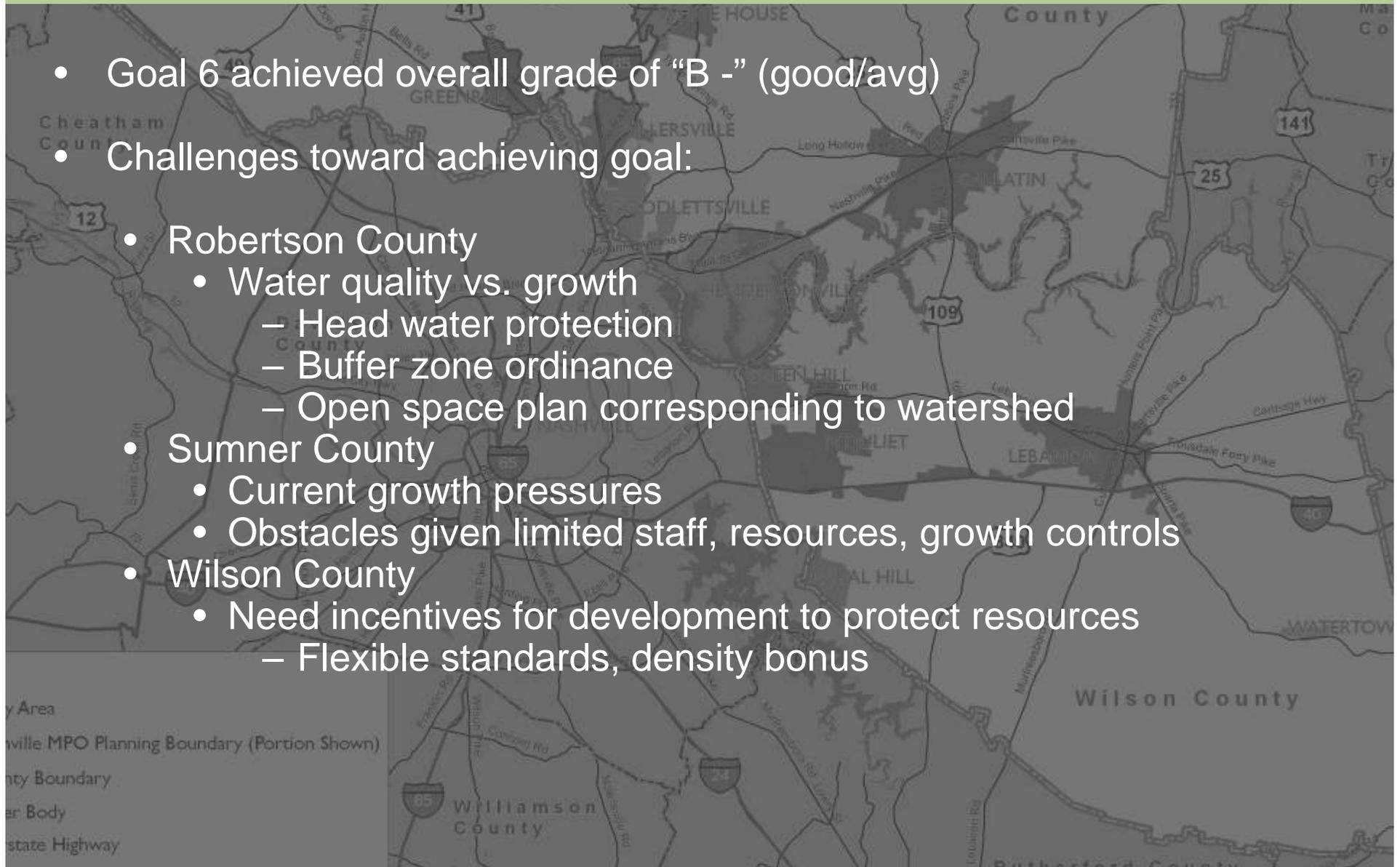
y Area
Nashville MPO Planning Boundary (Portion Shown)
County Boundary
County Body
State Highway

Goal 6: Protection of Natural Resources

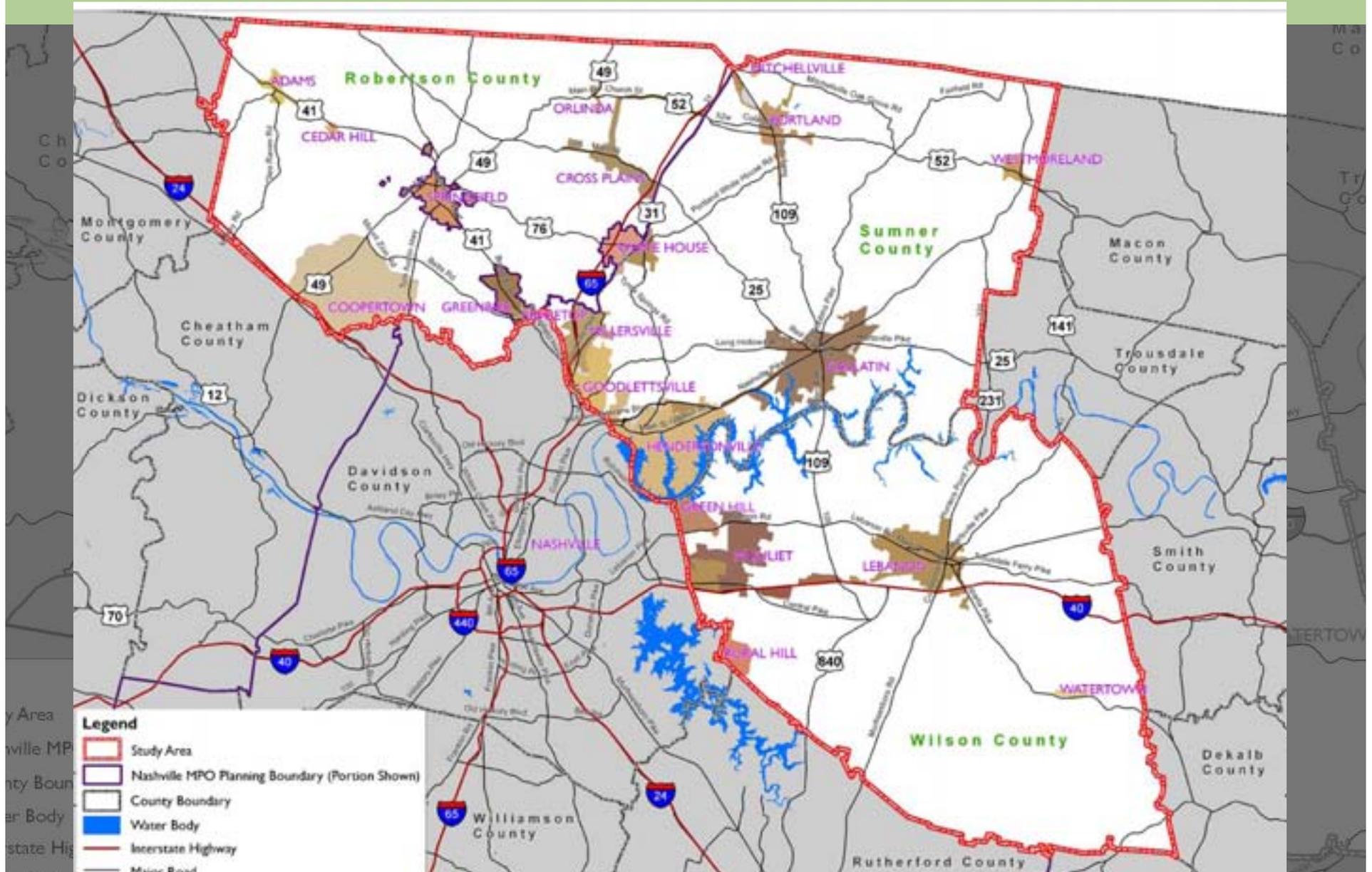


Goal 6: Protection of Natural Resources

- Goal 6 achieved overall grade of “B -” (good/avg)
- Challenges toward achieving goal:
 - Robertson County
 - Water quality vs. growth
 - Head water protection
 - Buffer zone ordinance
 - Open space plan corresponding to watershed
 - Sumner County
 - Current growth pressures
 - Obstacles given limited staff, resources, growth controls
 - Wilson County
 - Need incentives for development to protect resources
 - Flexible standards, density bonus



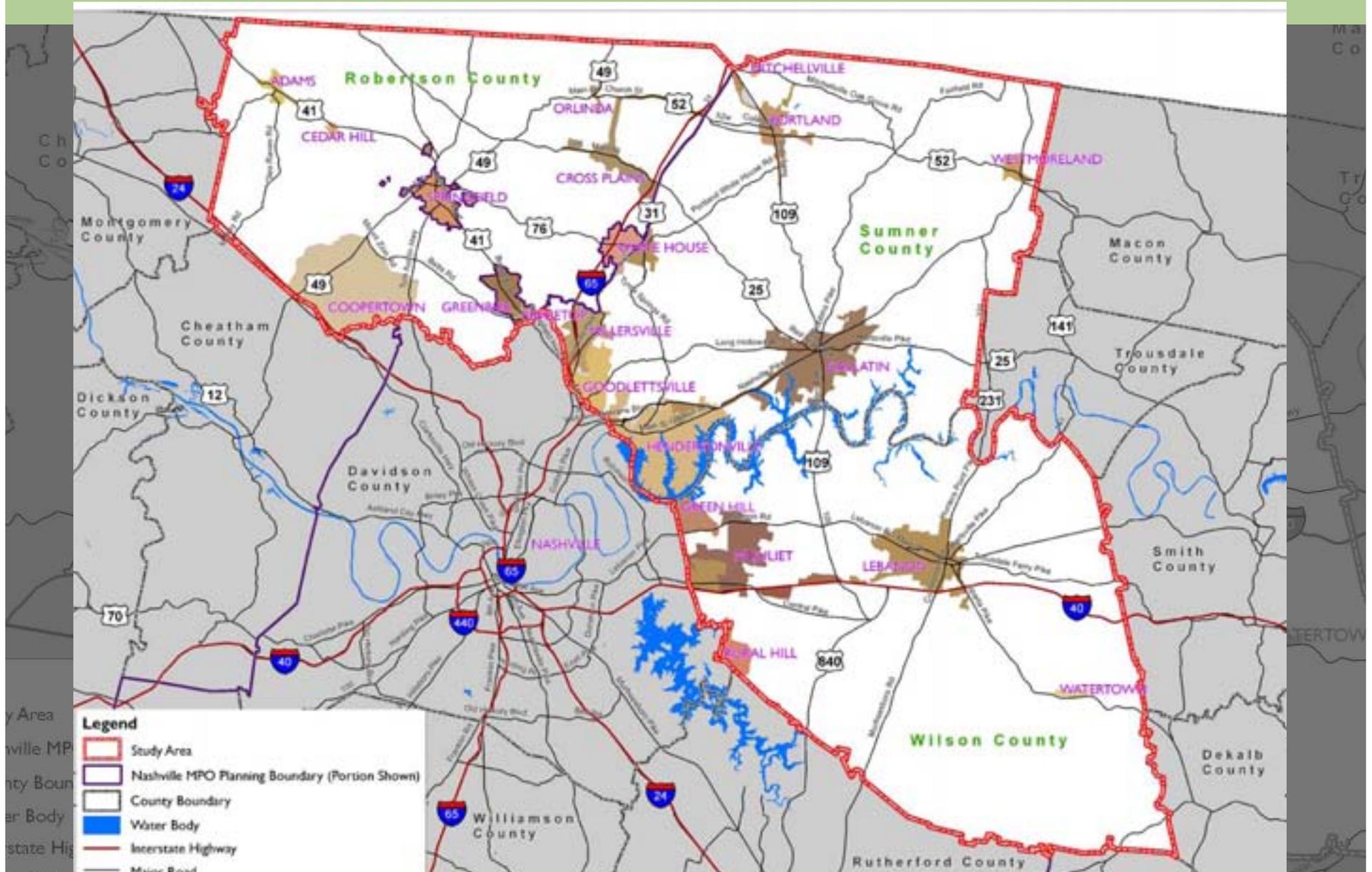
Goal 7: Efficient Transportation System



Goal 7: Efficient Transportation System

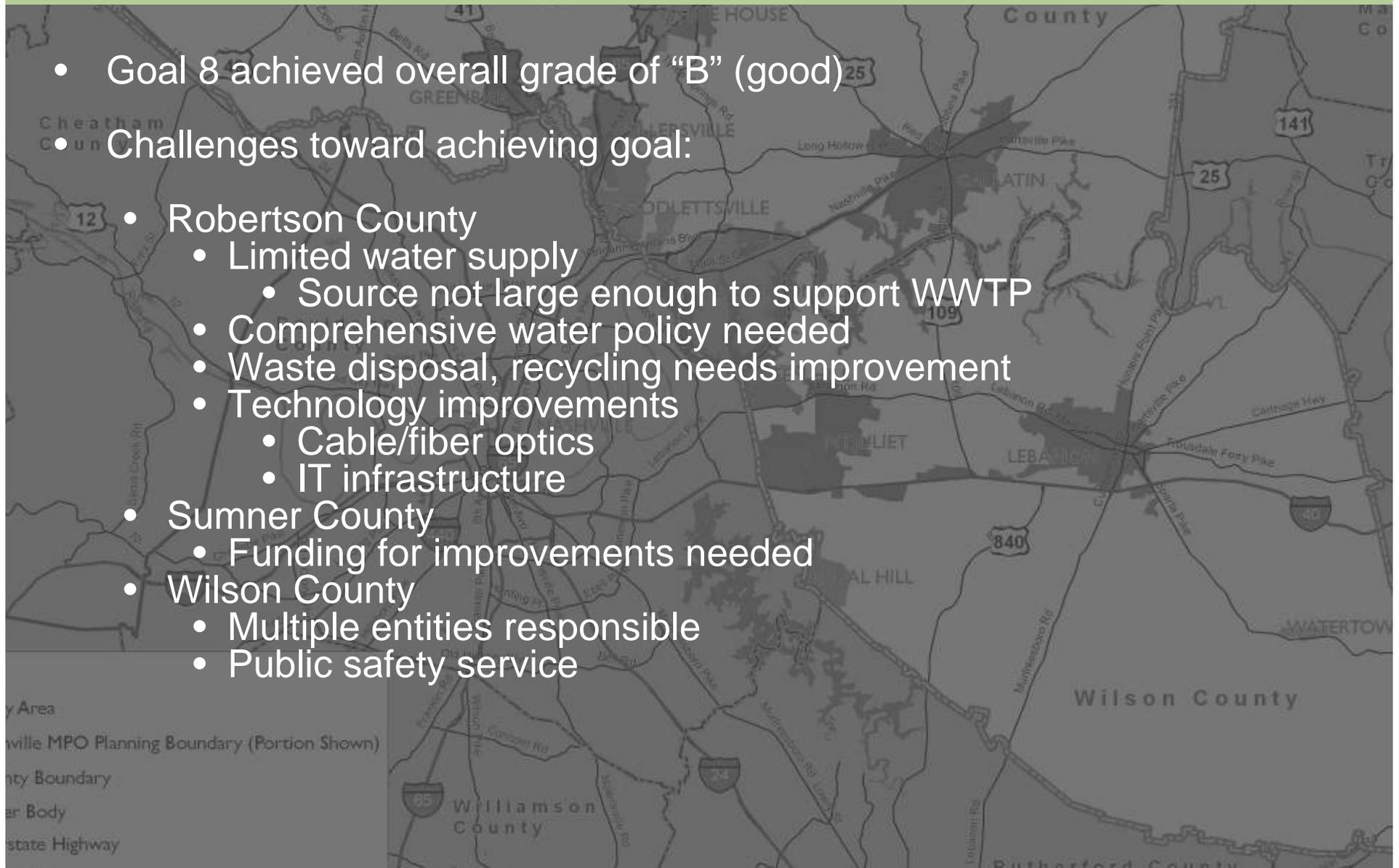
- Goal 7 achieved overall grade of “C” (average)
- Challenges toward achieving goal:
 - Robertson County
 - Lack of roadway infrastructure
 - To/from Nashville
 - Strong North/South
 - Weak East/West (lack of cross-county linkage)
 - Sumner County
 - Lack of funding for infrastructure
 - Congestion
 - Mass transit vs. current density trend
 - Wilson County
 - Lack of roadway infrastructure
 - Weak North/South
 - Coordination of timing of road improvements with growth
 - Freight movement

Goal 8: Ensure Availability of Services



Goal 8: Ensure Availability of Services

- Goal 8 achieved overall grade of “B” (good)
- Challenges toward achieving goal:
 - Robertson County
 - Limited water supply
 - Source not large enough to support WWTP
 - Comprehensive water policy needed
 - Waste disposal, recycling needs improvement
 - Technology improvements
 - Cable/fiber optics
 - IT infrastructure
 - Sumner County
 - Funding for improvements needed
 - Wilson County
 - Multiple entities responsible
 - Public safety service



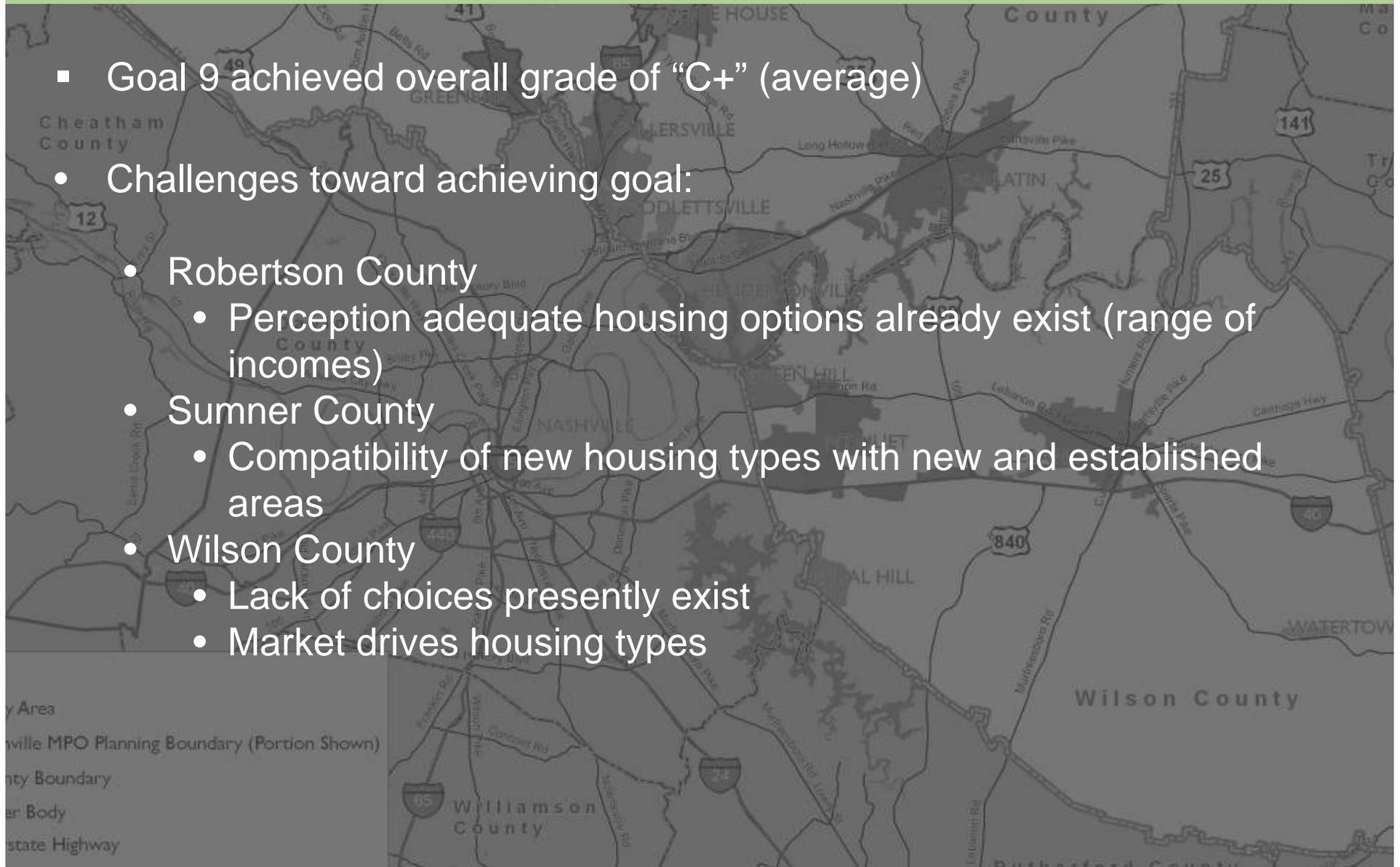
Goal 9: Provide Housing Options



Williamson MPO Planning Boundary (Portion Shown)
County Boundary
Water Body
State Highway

Goal 9: Provide Housing Options

- Goal 9 achieved overall grade of “C+” (average)
 - Challenges toward achieving goal:
 - Robertson County
 - Perception adequate housing options already exist (range of incomes)
 - Sumner County
 - Compatibility of new housing types with new and established areas
 - Wilson County
 - Lack of choices presently exist
 - Market drives housing types

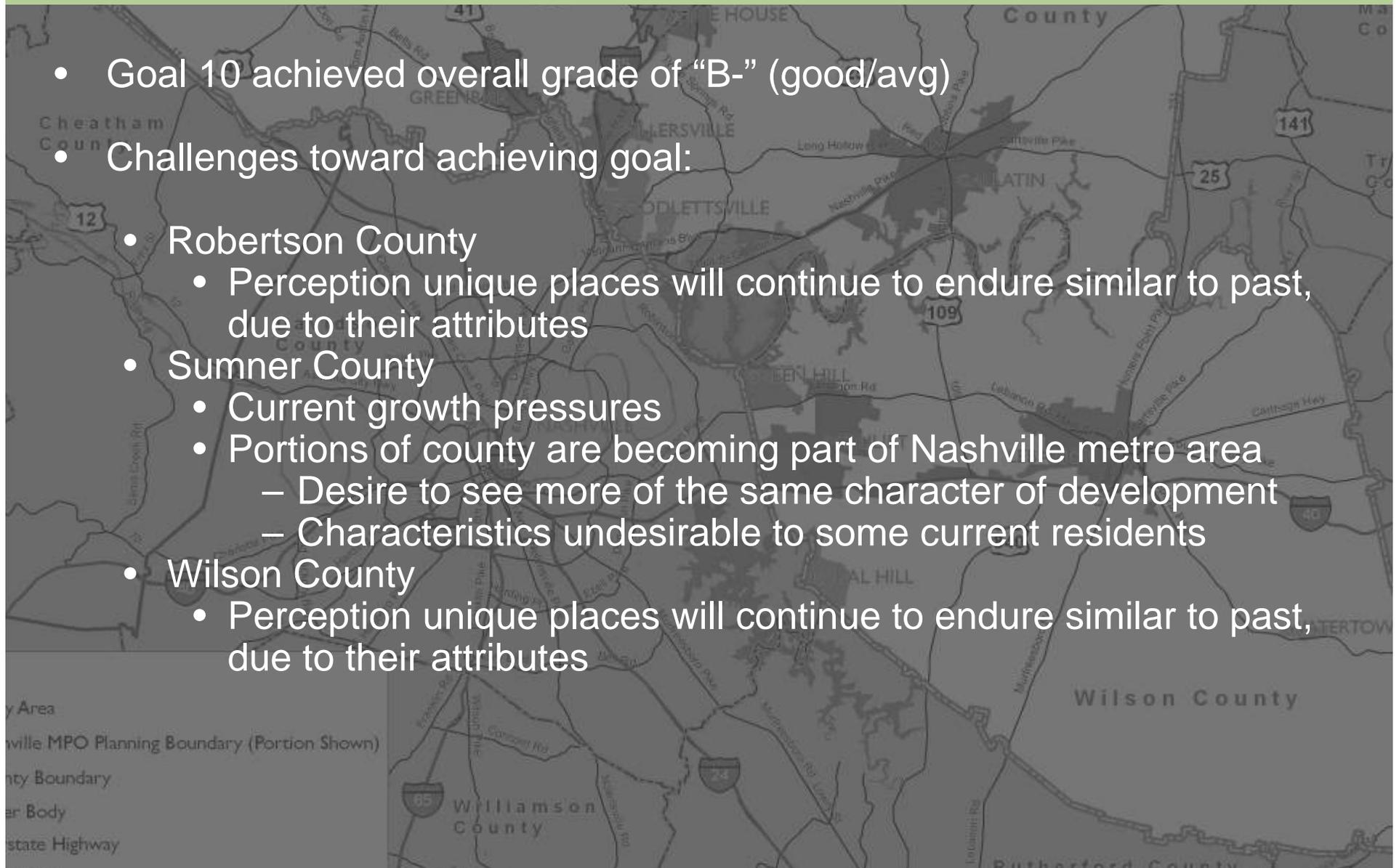


Goal 10: Maintain Sense of Community and Sense of Place



Goal 10: Maintain Sense of Community and Sense of Place

- Goal 10 achieved overall grade of “B-” (good/avg)
- Challenges toward achieving goal:
 - Robertson County
 - Perception unique places will continue to endure similar to past, due to their attributes
 - Sumner County
 - Current growth pressures
 - Portions of county are becoming part of Nashville metro area
 - Desire to see more of the same character of development
 - Characteristics undesirable to some current residents
 - Wilson County
 - Perception unique places will continue to endure similar to past, due to their attributes



Quality Growth: Choices



Precedent Regional Efforts

- Pierce Report (1999)
- Regional Planning Summit Proceedings (1999)
- Cumberland Region Tomorrow
 - Report to the Region (2003)
 - Lincoln Institute of Land Policy Report (2006)
 - Quality Growth Toolbox (2006)
- TDOT PlanGo (2005)
- Nashville Civic Design Center
 - The Plan of Nashville
- Tennessee Growth Readiness
- Cumberland River Compact

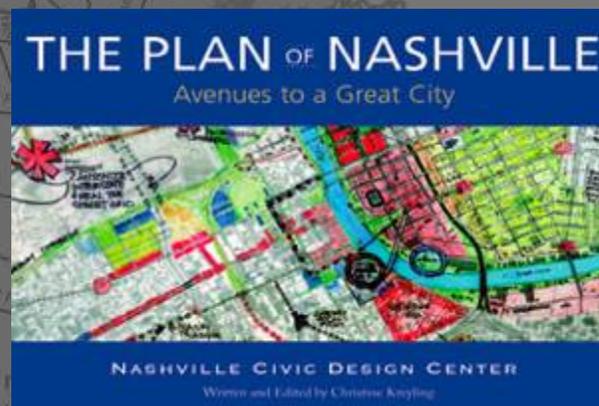


TENNESSEE GROWTH READINESS PROGRAM

Water Quality Matters

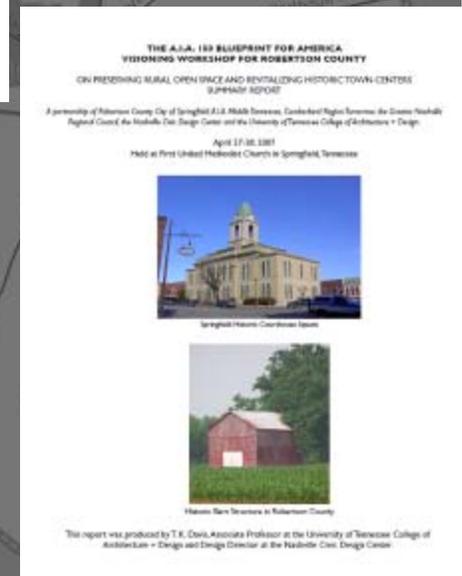
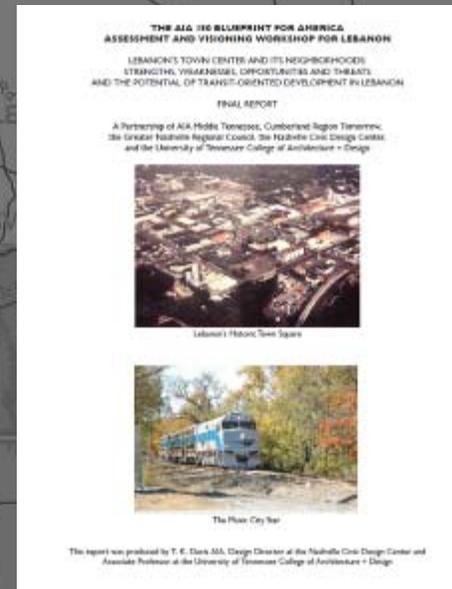
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Precedent Local Efforts

- AIA 150 Blueprint for America
- Visioning workshops in Lebanon, Robertson County



Results of Group Exercise – Report Card on BAU

Goals	Grade
Historic Conservation and Enhancement	B-
Viable Agriculture	B
Rural Preservation	C+
Economic Enrichment while Safeguarding Existing Public and Private Development	B-
Preserve Urban Centers	B
Protection of Natural Resources	B-
Efficient Transportation System	C
Ensure Availability of Services	B
Provide Housing Options	C+
Maintain Sense of Community and Sense of Place	B-

Choices: Tools Available



	Regional visioning	Regional open space, parks, rec and greenway plans	Regional infrastructure plans (water, wastewater)	Regional stormwater management policies	Comprehensive Plan / Land Use Plan	Comprehensive Transportation Plan	Small area plans / revitalization / redevelopment plans	Capital improvement program / plans for community infrastructure	Urban growth boundaries	Strategic planning	Education and community involvement efforts	Form-based zoning (with flexible standards)	Design guidelines	Local historic district / guidelines	Conservation easements	Programs / assistance supporting agricultural, other types of businesses	Agricultural districts / easements
Historic Conservation and Enhancement					X						X	X		X			
Viable Agriculture					X				X	X	X		X			X	X
Rural Preservation	X	X	X		X				X		X	X	X		X	X	X
Economic Enrichment while Safeguarding Existing Public and Private Development					X		X	X		X						X	
Preserve Urban Centers					X		X		X			X	X	X			
Protection of Natural Resources	X	X		X	X				X		X		X		X		
Efficient Transportation System	X				X	X	X	X	X	X	X		X				
Ensure Availability of Services			X	X	X			X	X	X							
Provide Housing Options					X					X		X	X				
Maintain Sense of Community and Sense of Place	X	X			X		X		X		X	X	X	X			

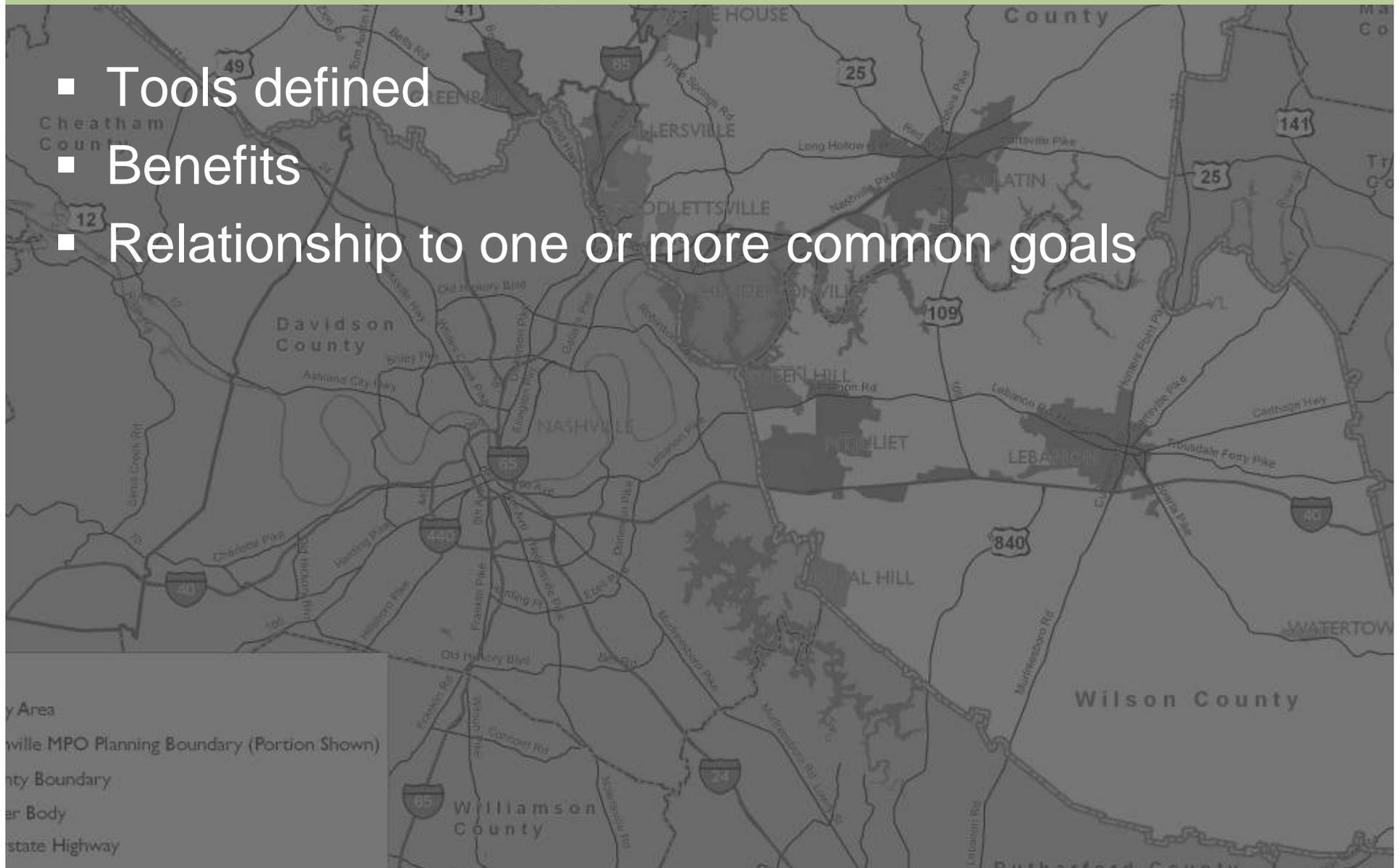


Choices: Tools Available

	Rural conservation development forms (towns, villages, hamlet, conservation subdivisions)	Administrative review in lieu of public hearing	Expedited development review process	Reduced permitting / plan review fees	Public recognition	Wildlife, habitat and natural resources management plan	Affordable housing programs (state and local)	Adaptive reuse	Impact fees	Tax increment financing	Tax credits	Density and Intensity bonus	Mixed-use / Infill development	Partnerships (organizations, not-for-profit)
Historic Conservation and Enhancement								X			X			X
Viable Agriculture	X				X						X		X	X
Rural Preservation	X				X						X		X	X
Economic Enrichment while Safeguarding Existing Public and Private Development	X									X		X	X	X
Preserve Urban Centers								X		X			X	X
Protection of Natural Resources	X	X	X	X	X	X			X		X	X	X	X
Efficient Transportation System									X	X		X	X	
Ensure Availability of Services									X				X	
Provide Housing Options	X						X	X			X	X	X	X
Maintain Sense of Community and Sense of Place								X					X	X

Choices: Tools Available

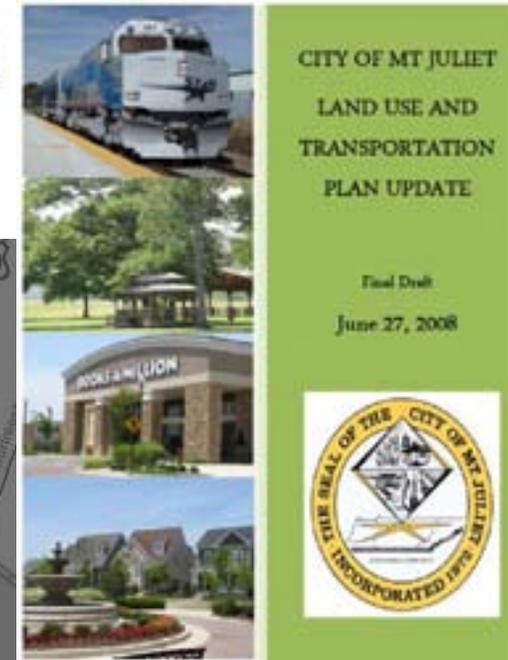
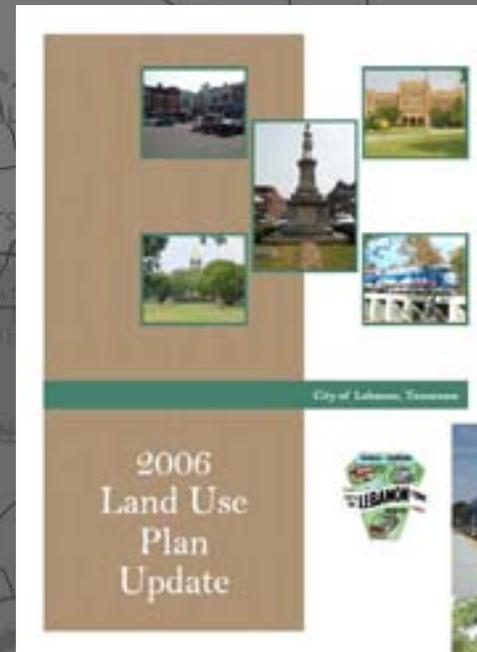
- Tools defined
- Benefits
- Relationship to one or more common goals



Choices: Tools Available

REGIONAL AND LOCAL PLANS

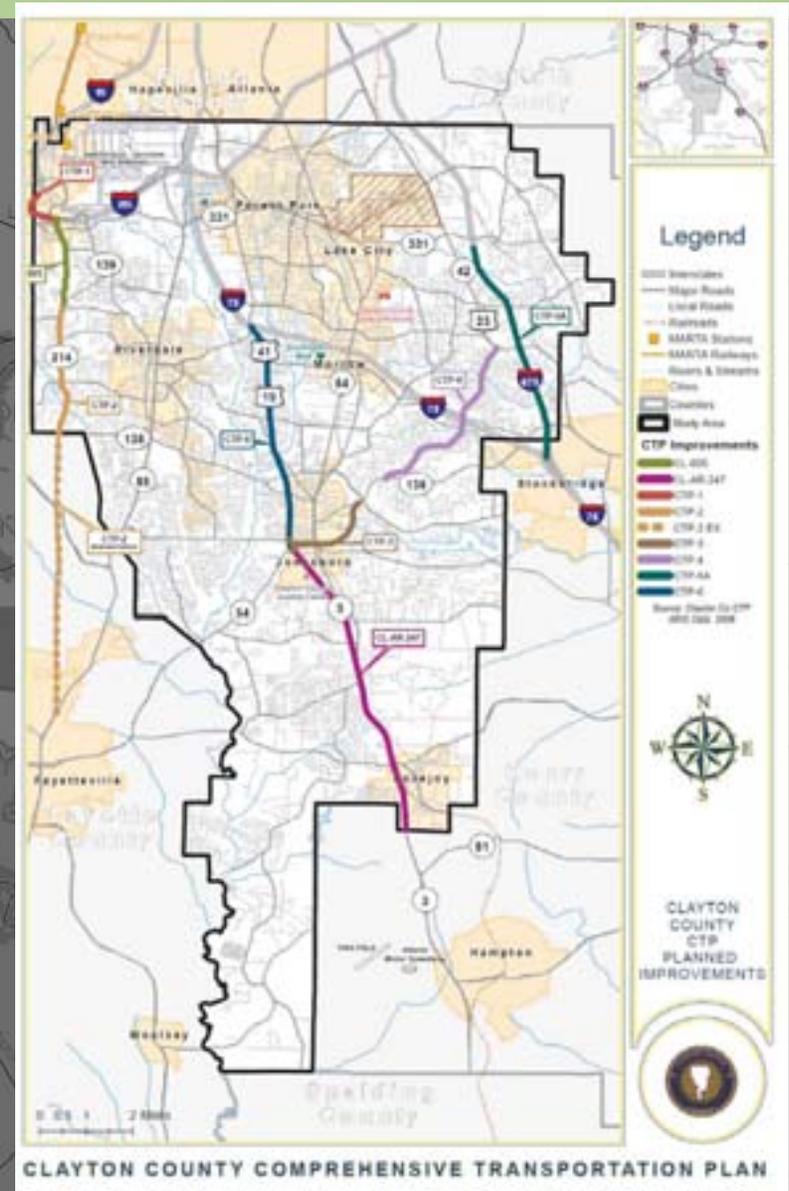
- Comprehensive Plan / Land Use Plan
 - Typically begins with visioning
- Potential Comp Plan Elements
 - Future Land Use / Land Use Plan
 - Transportation / CTP
 - Environmental
 - Cultural / Historic
 - Economic Development
 - Housing
 - Open Space, Parks and Rec
 - Water
 - Wastewater
 - Solid Waste
 - Stormwater
 - Emergency Services
 - Education
 - Capital Improvements
- Typically 15-20 year timeframe, updated every 5 years
- Implemented by tools
 - Small Area Plans, specific Regulations



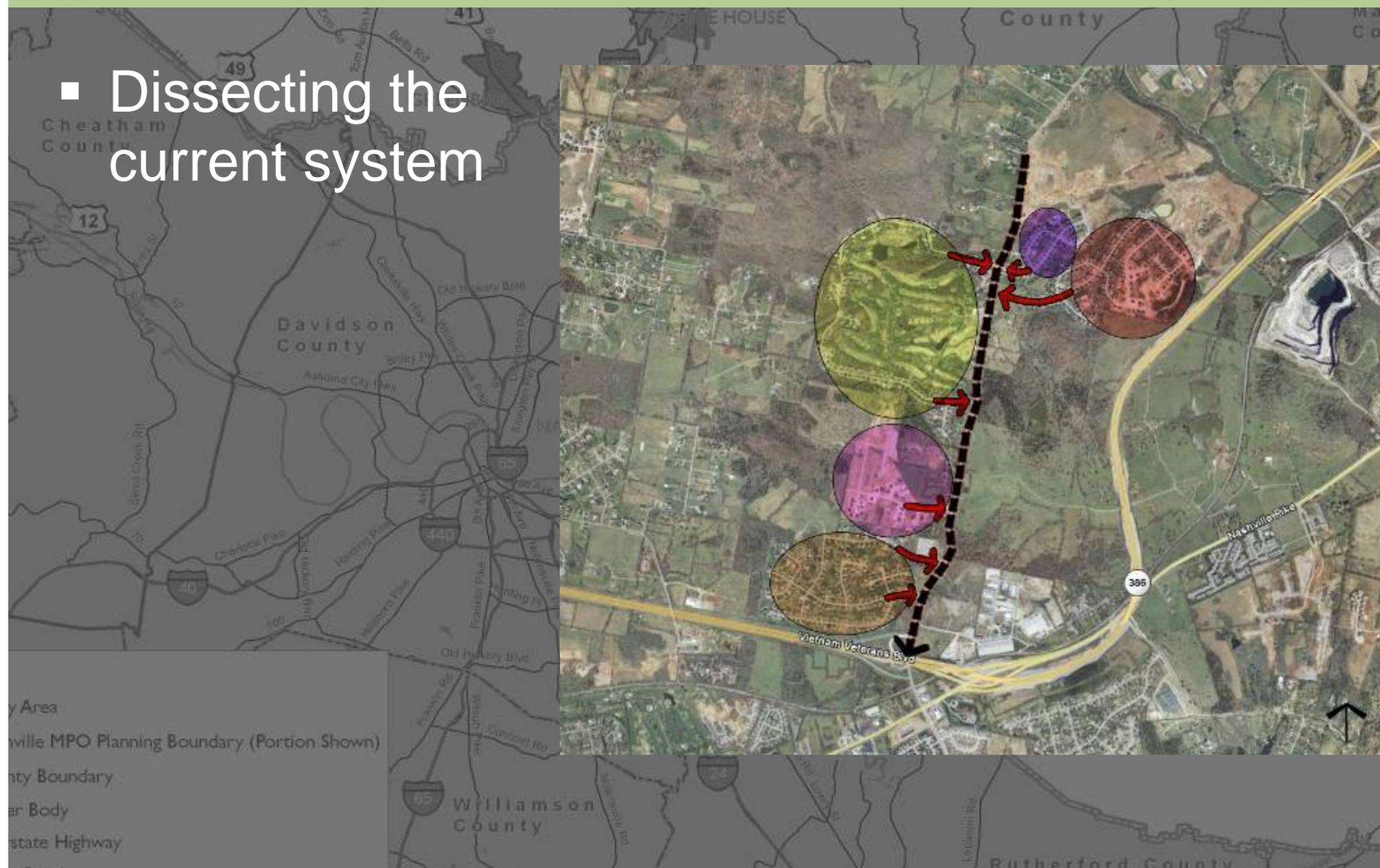
Choices: Tools Available

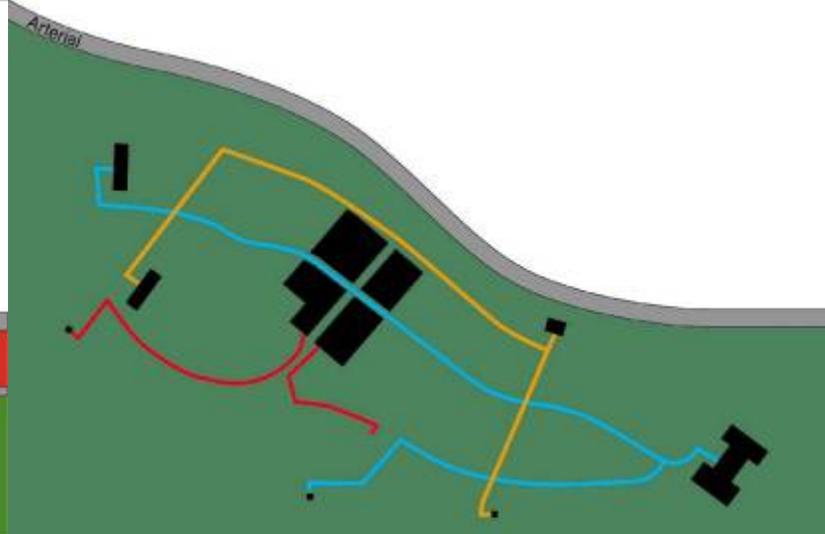
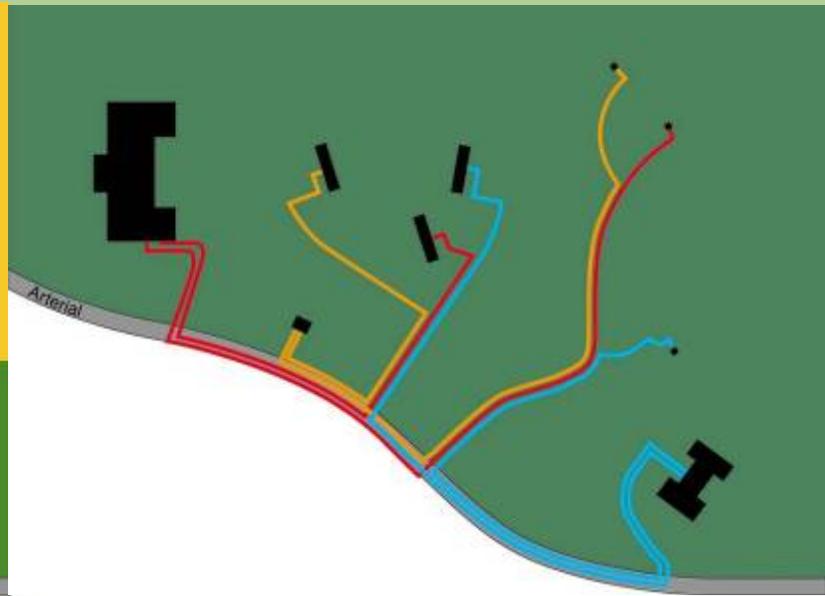
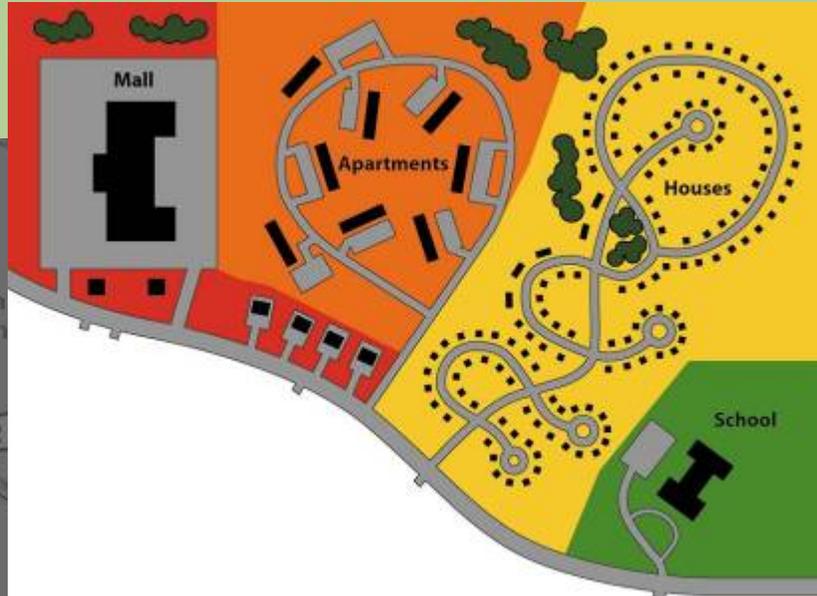
REGIONAL AND LOCAL PLANS

- Comprehensive Transportation Plan
 - Address Entire Hierarchy of Street Systems
 - Alternative Modes of Transportation

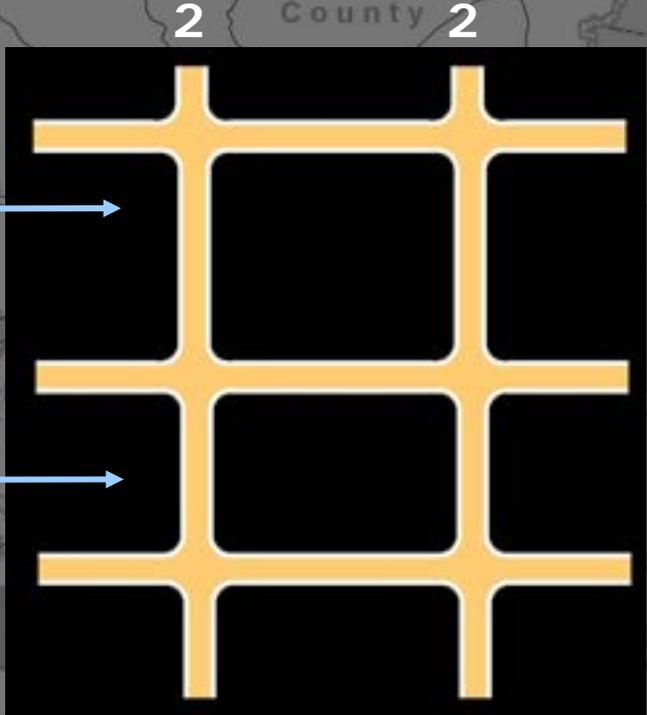
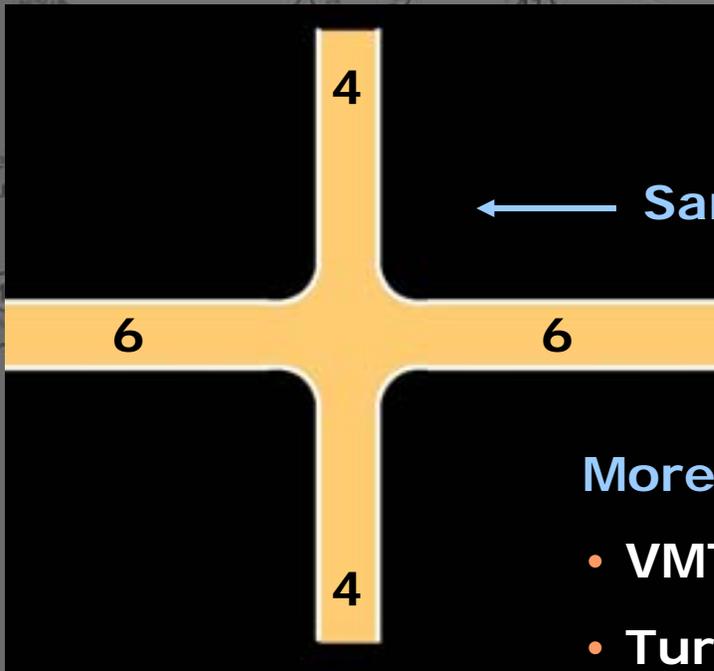


- Dissecting the current system





... a connected street system can get short trips off of the arterial highway, allowing it to handle longer trips . . .



← Same Total Lanes →

→ More Capacity →

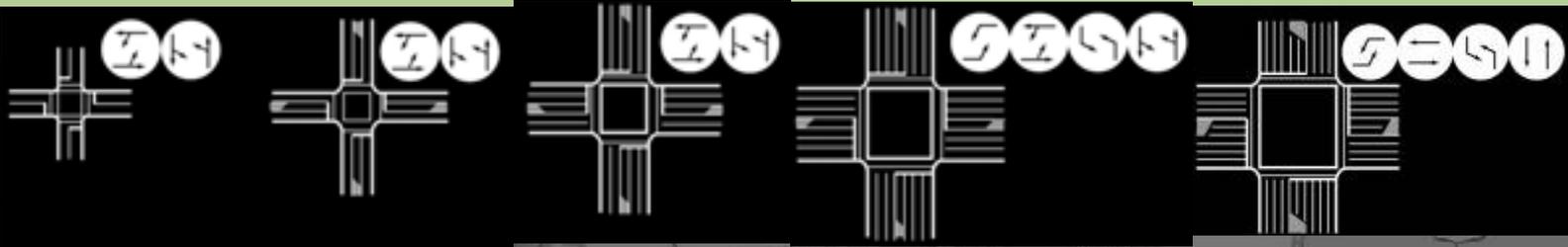
- VMT
- Turns
- Clearance Time
- Signal Phase

... The arithmetic of traffic capacity is not always intuitive ...

$$(2 + 2) > 4$$

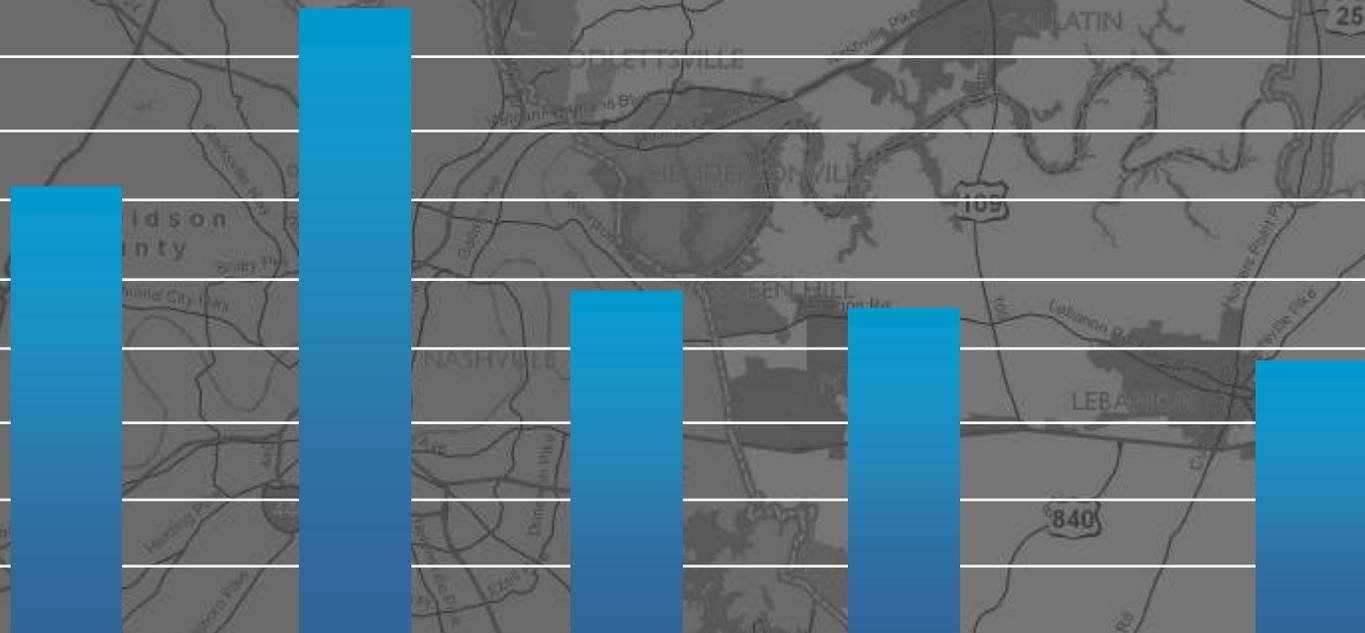


Cheatham County

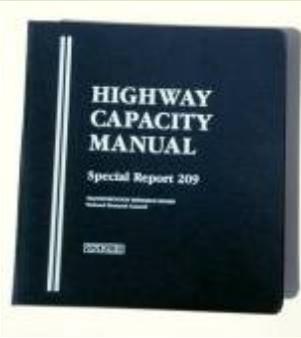


Capacity of Additional Through Lane (VPH)

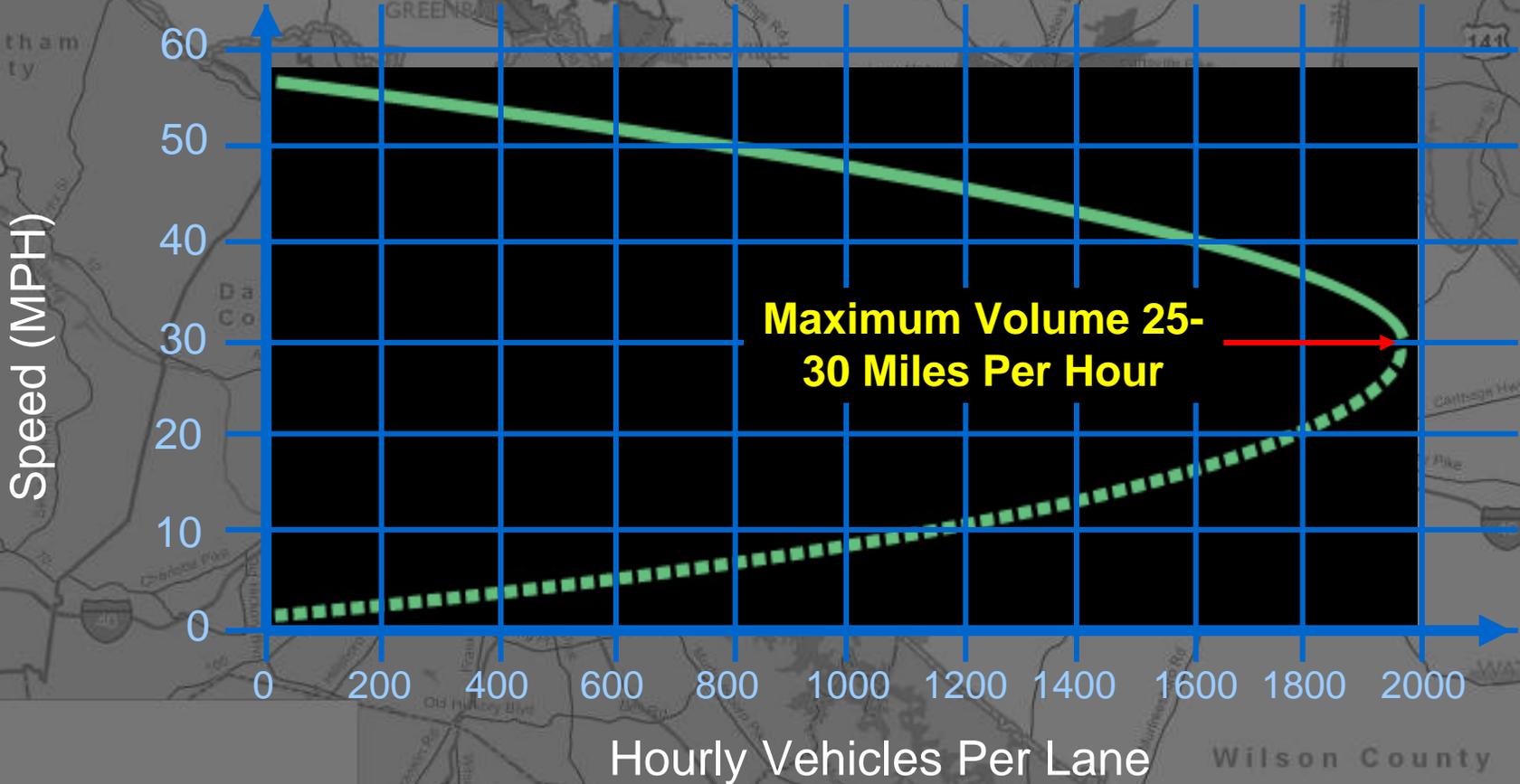
800
600
400
200



... The arithmetic of traffic capacity is not always intuitive ...
Lane efficiency peaks with a 3-lane street



Speed – Flow Relationship



... The arithmetic of traffic capacity is not always intuitive ...
The maximum volume can be handled at 25-30 mph

REGIONAL AND LOCAL PLANS



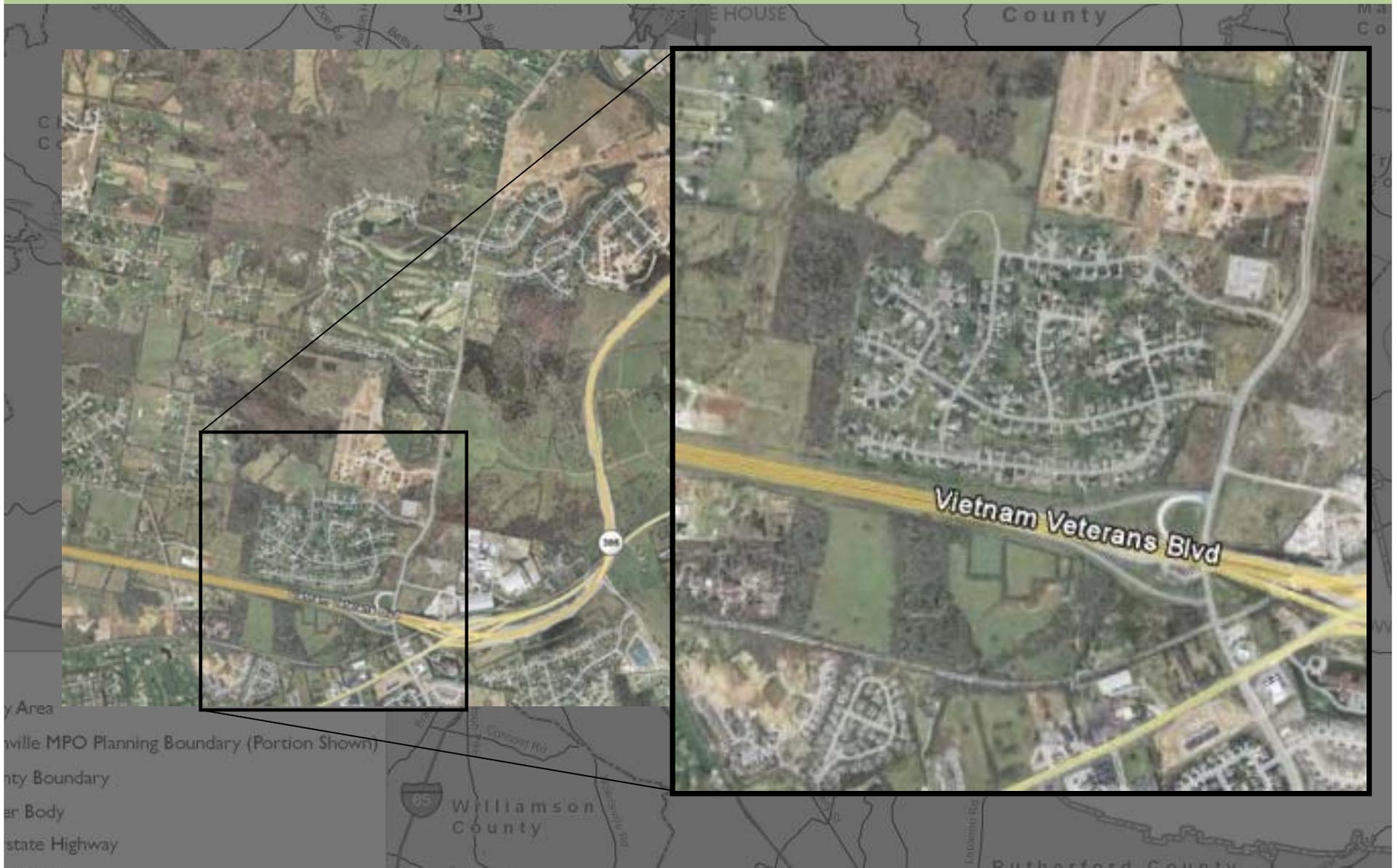
REGIONAL AND LOCAL PLANS

Density Level (In Units per Acre)	Service
4–6	Minimal bus service (approx. 1 bus / hour), subsidized Marginal multimodal potential
7–8	Intermediate bus service (30 minute headway)
9–10	Light Rail (5 min. peak headway) and feeder buses
12	Rapid transit (5 min. peak headway)
15	Frequent bus service (120 /day) High multi-modal potential

Sources:

- Campoli, Julie, MacLean, Alex, "Visualizing Density," 2002.
- Holtzclaw, John, "Community Characteristics Promoting Transit and Walking," (from "Using Residential Patterns and Transit To Decrease Auto Dependence and Costs"; Natural Resources Defense Council, June, 1994).
- Transportation Districts and Areawide Level of Service Handbook. Systems Planning Office, Florida Department of Transportation, June 28, 2002.

REGIONAL AND LOCAL PLANS





Atlanta, GA 4.8 units / acre



context



neighborhood



vertical



I'ron, Mt. Pleasant, SC 8.8 units / acre



context



neighborhood



vertical

Choices: Tools Available

REGIONAL AND LOCAL PLANS

- Mixed-use
- Infill development

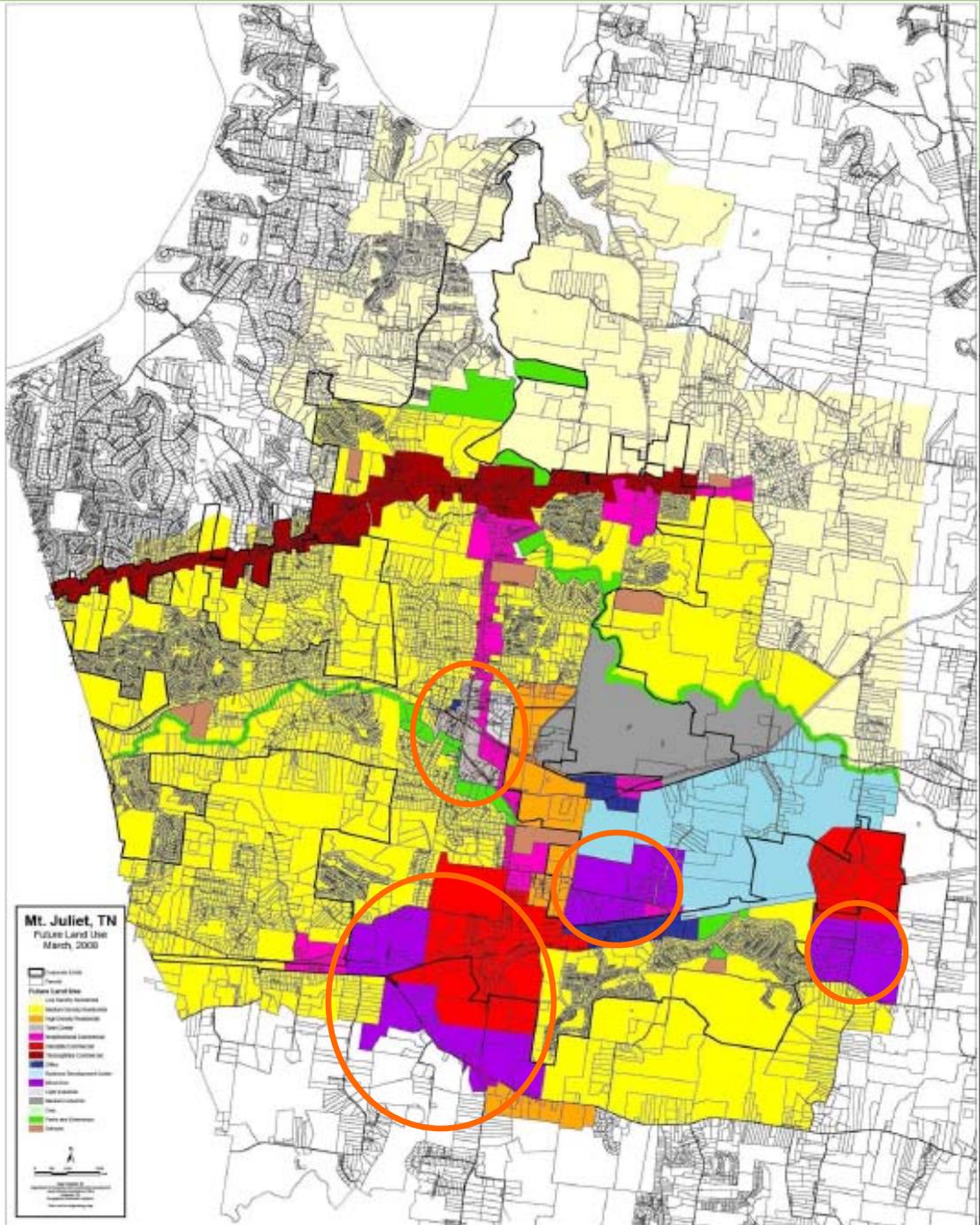


REGIONAL AND LOCAL PLANS





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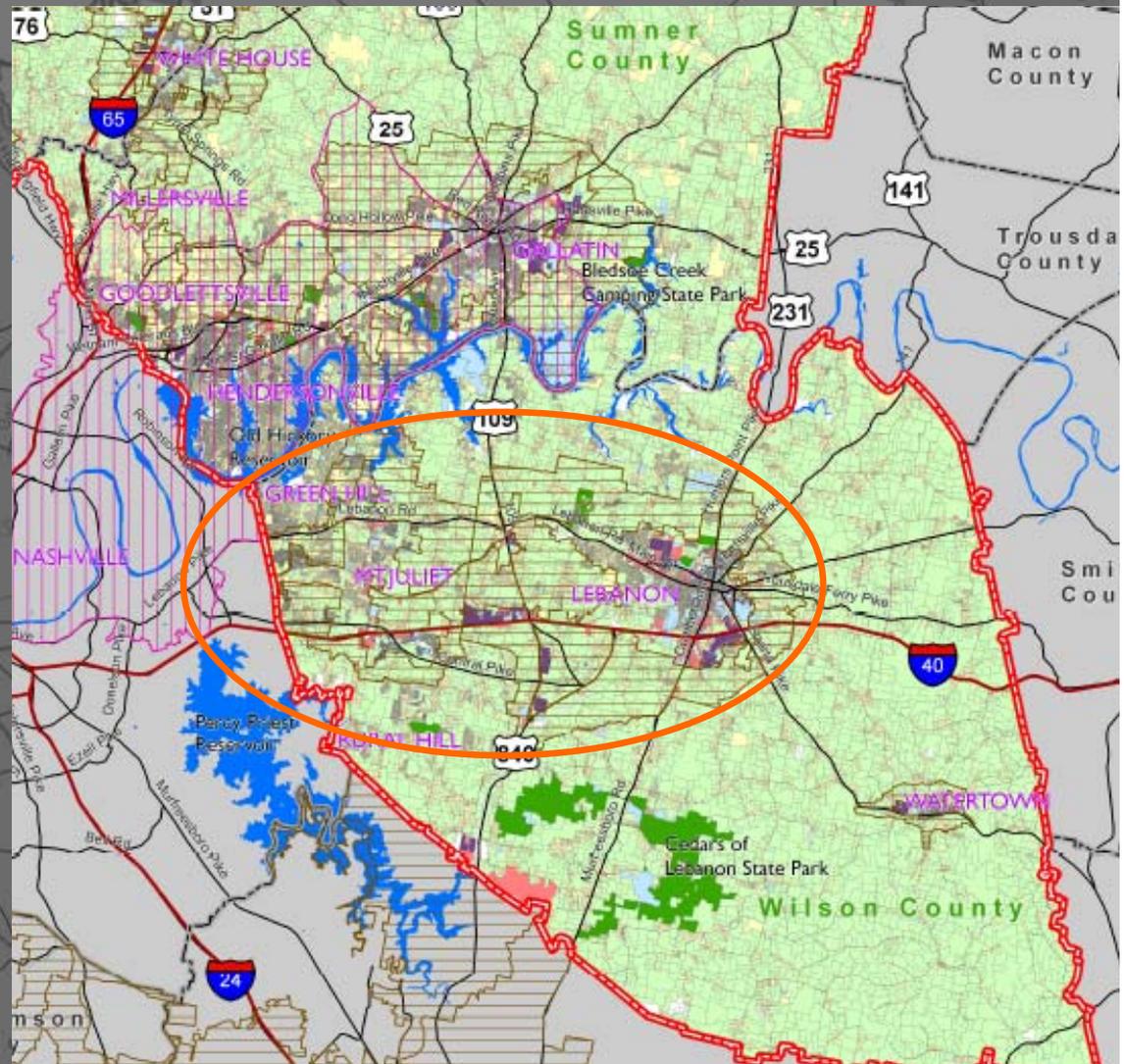


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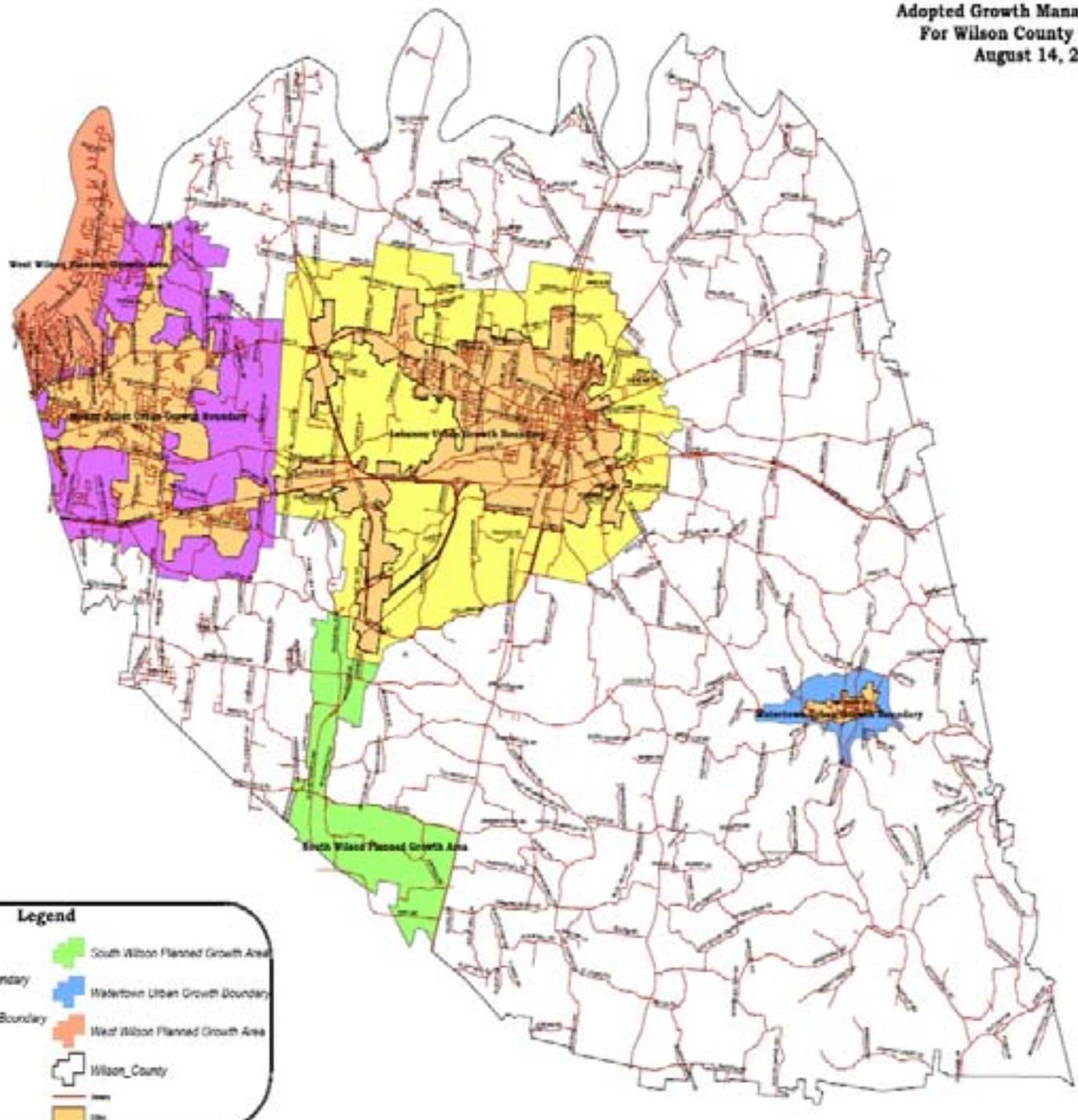
Choices: Tools Available

REGIONAL AND LOCAL PLANS

- Urban growth boundaries
 - Planned Growth Areas



Adopted Growth Management Plan
For Wilson County Tennessee
August 14, 2000



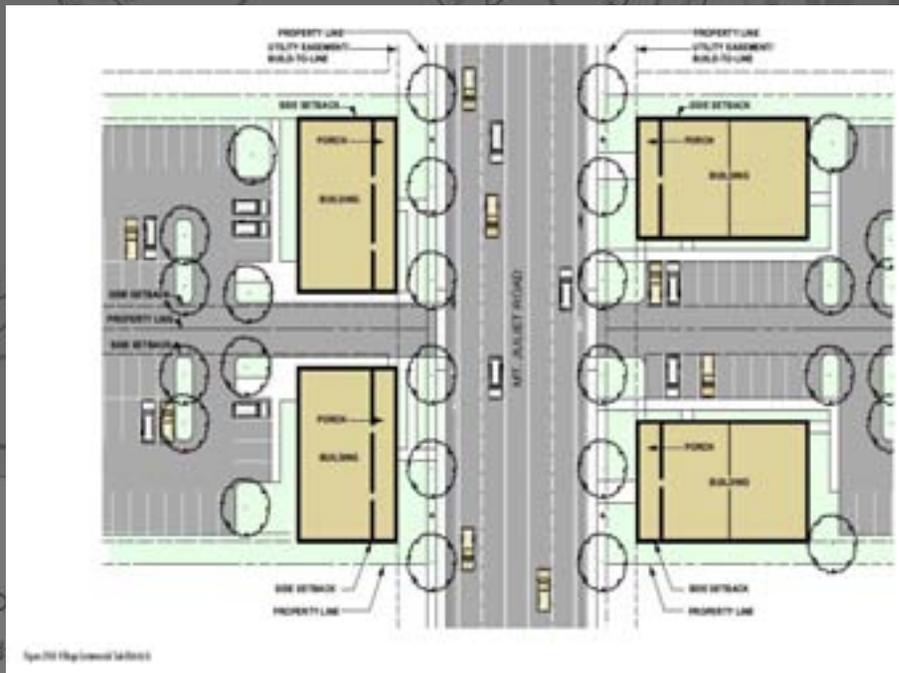
Legend

Area	Description
	Lebanon Urban Growth Boundary
	Mount Juliet Urban Growth Boundary
	South Wilson Planned Growth Area
	WaterTown Urban Growth Boundary
	West Wilson Planned Growth Area
	Wilson County
	Major Road
	Other Road

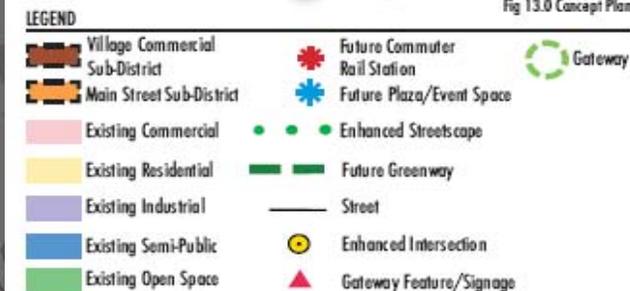
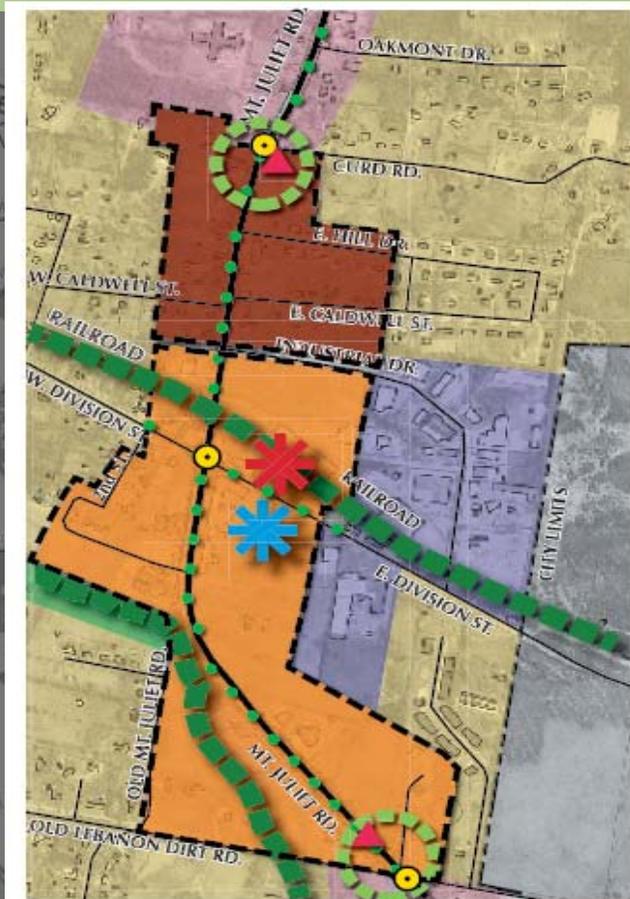


Choices: Tools Available

- Revitalization Plans
 - Implemented using zoning, design standards (regulatory)



REGIONAL AND LOCAL PLANS



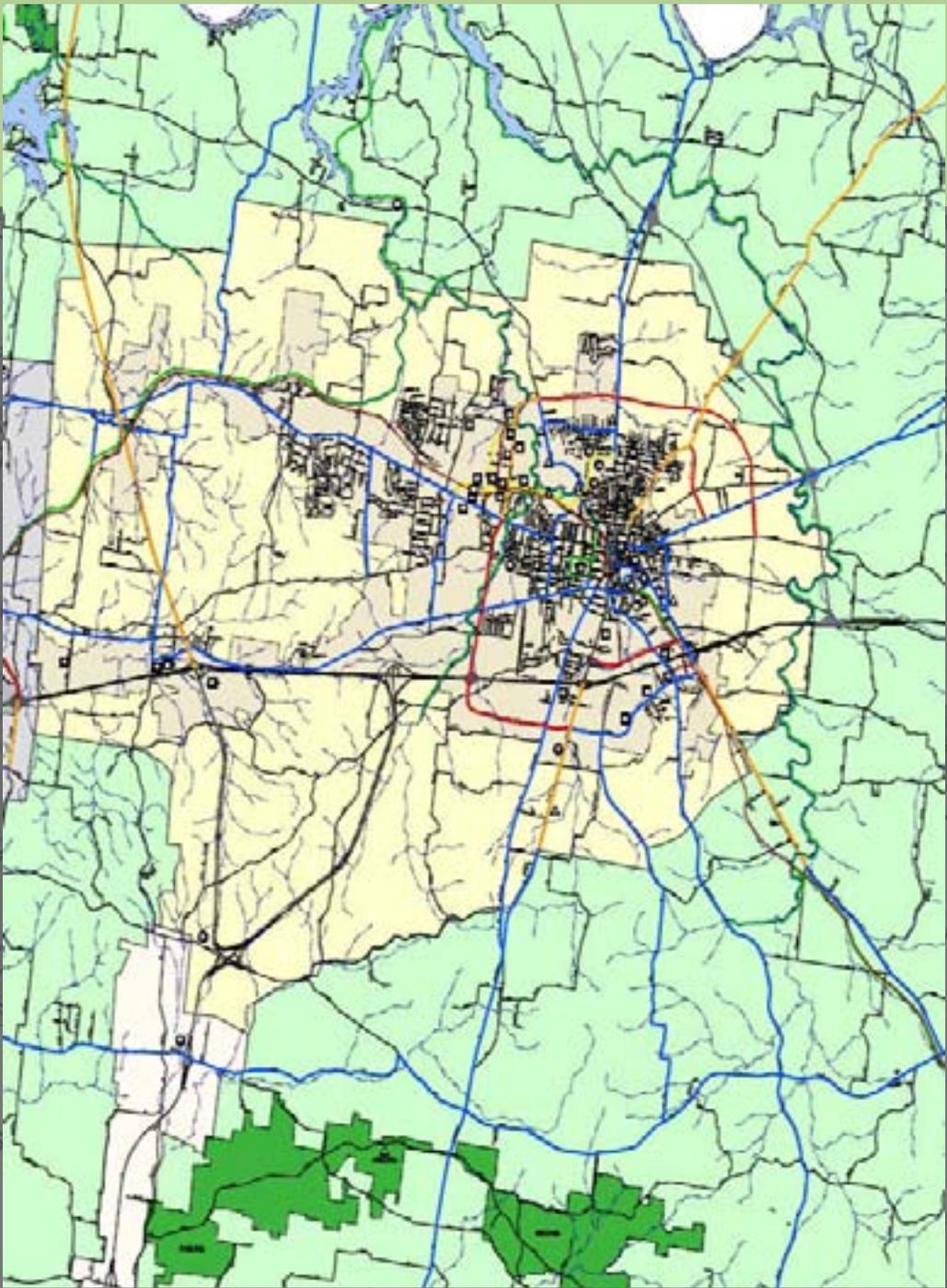
Choices: Tools Available

REGIONAL AND LOCAL PLANS

- Open space, parks, rec and greenway plans



Photos: Robertson County



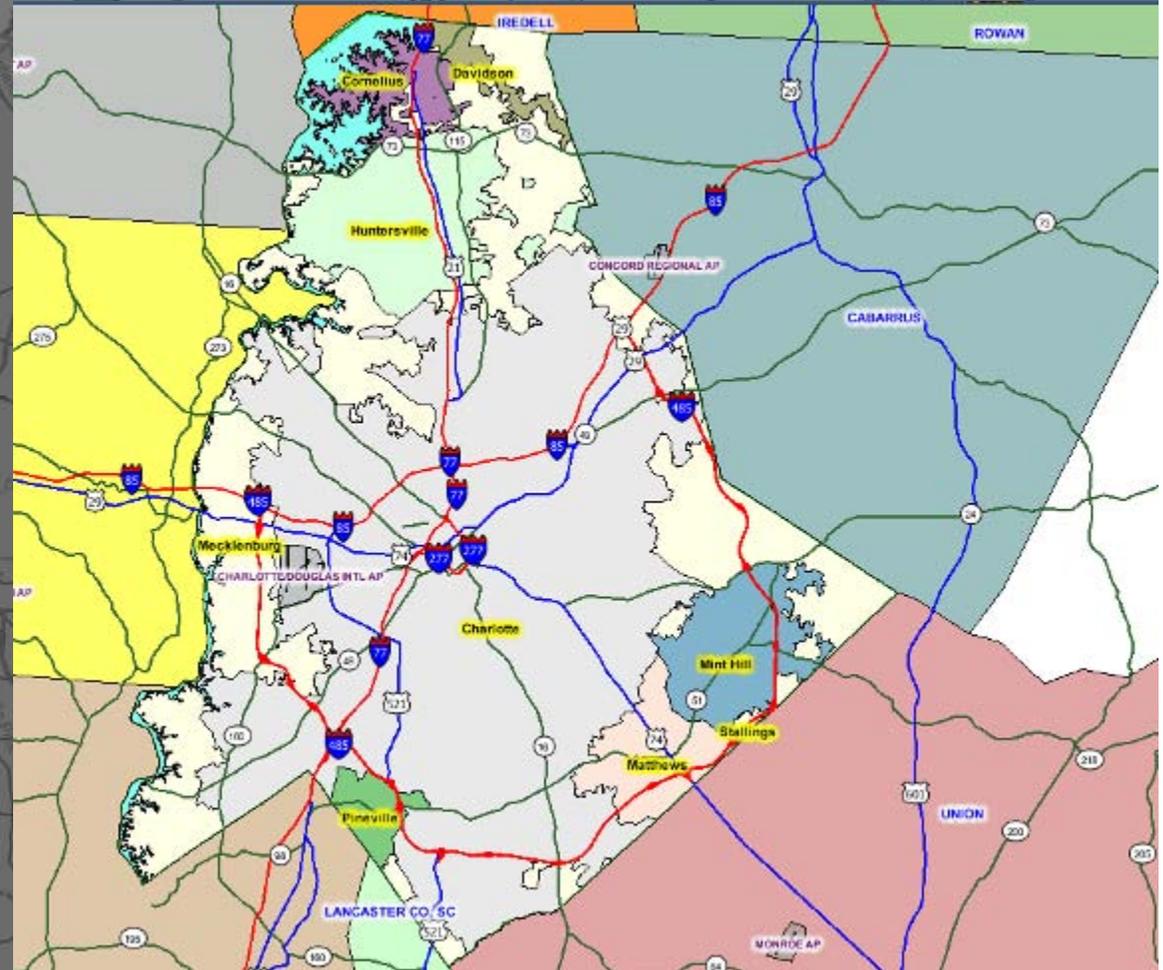
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Wilson County
Rutherford County

Choices: Tools Available

REGIONAL AND LOCAL PLANS

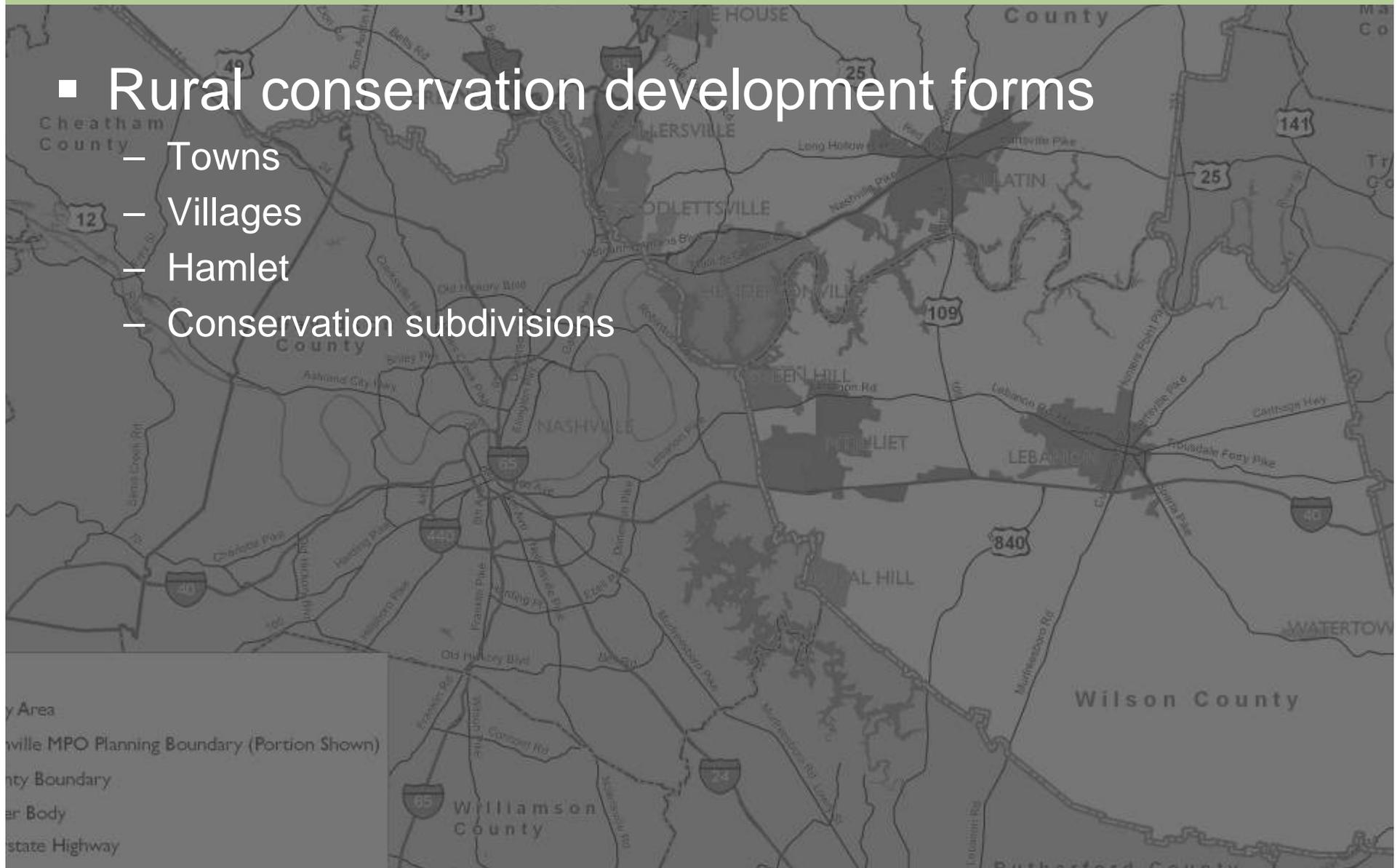
- Regional infrastructure plans
 - water, wastewater
- Regional stormwater management policies



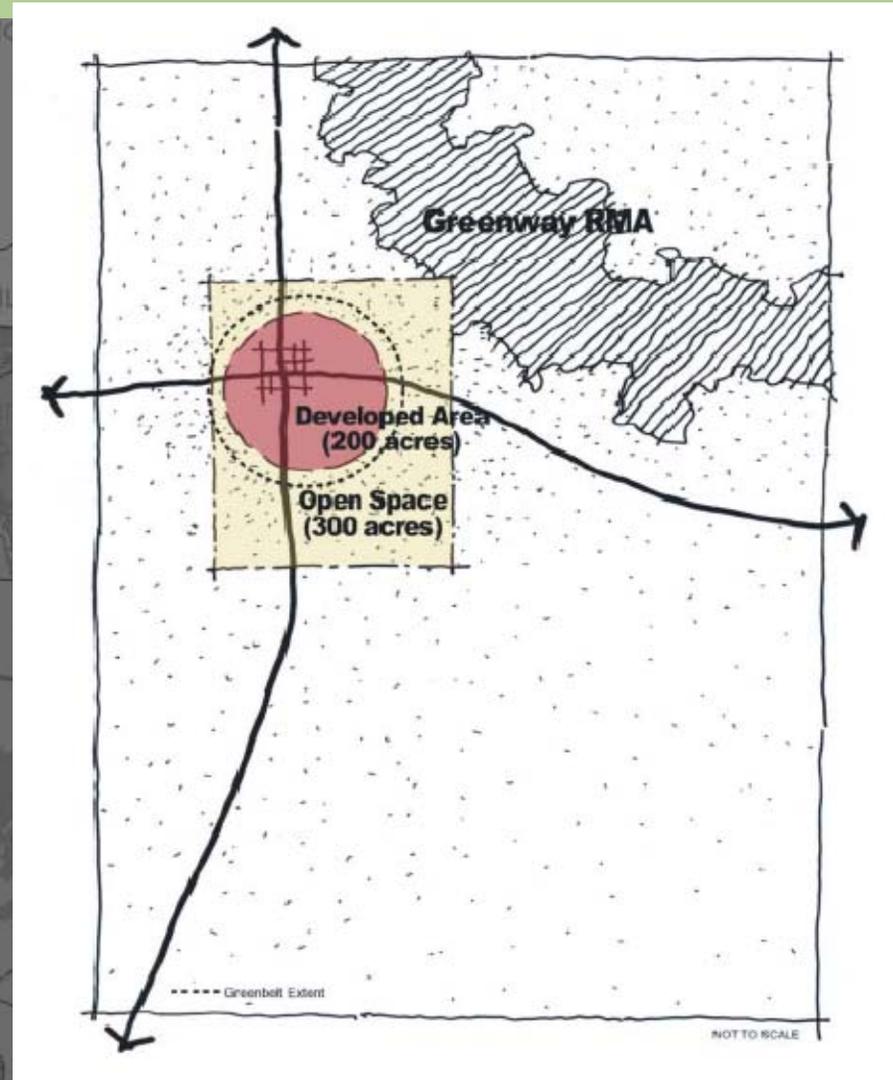
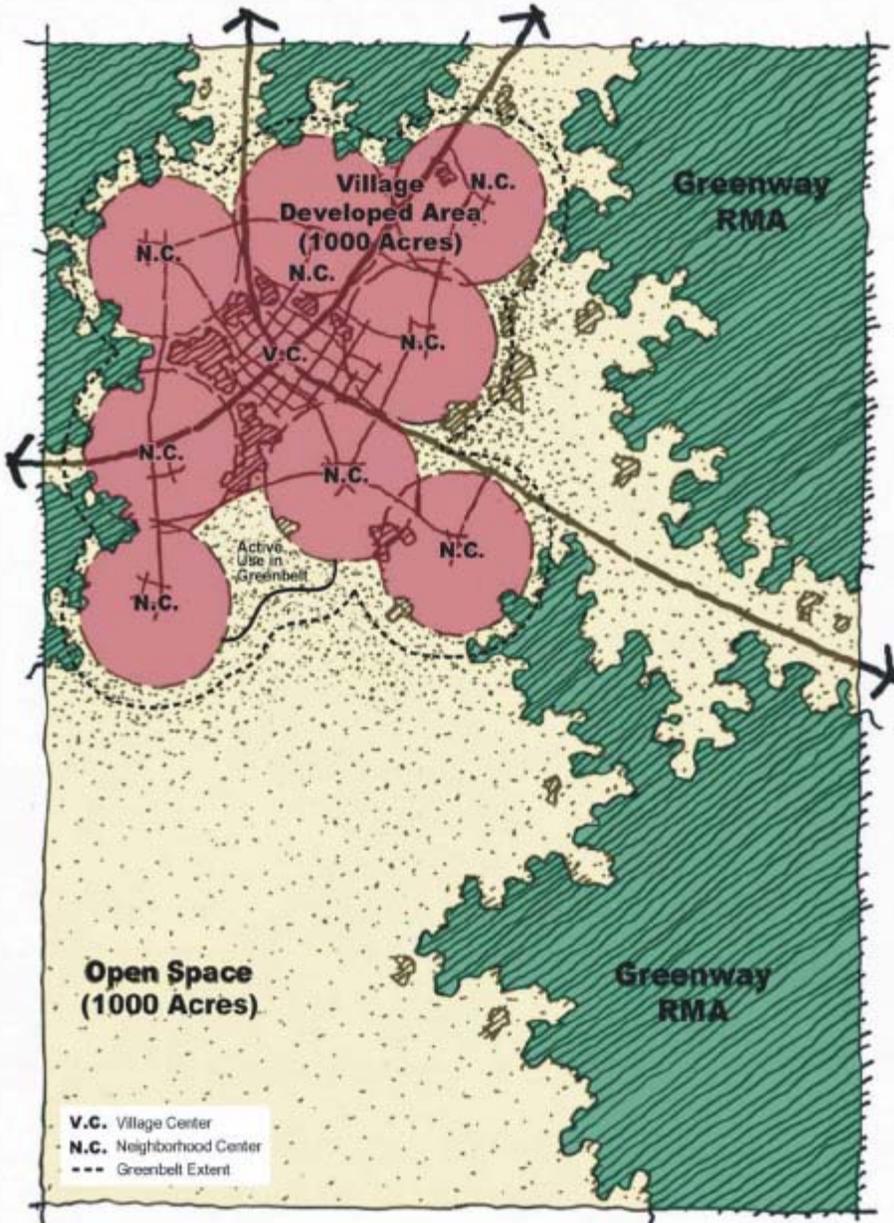
Choices: Tools Available

DESIGN

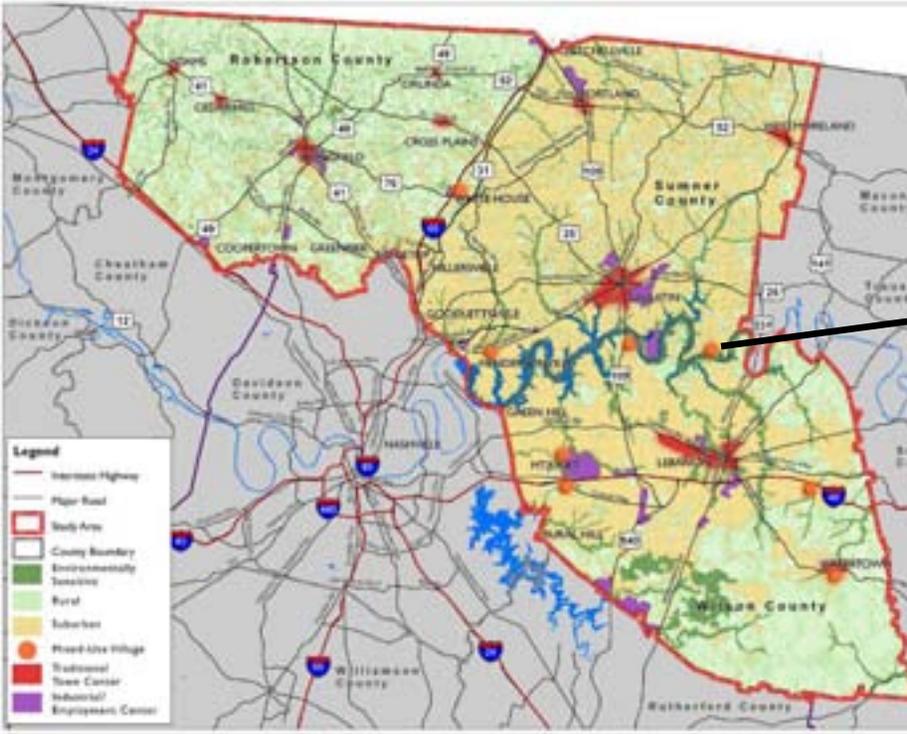
- Rural conservation development forms
 - Towns
 - Villages
 - Hamlet
 - Conservation subdivisions



DESIGN



REGULATORY



CHARACTER AREA MAP
Tri-County Transportation and Land Use Study
Robertson, Sumner, and Wilson Counties, Tennessee

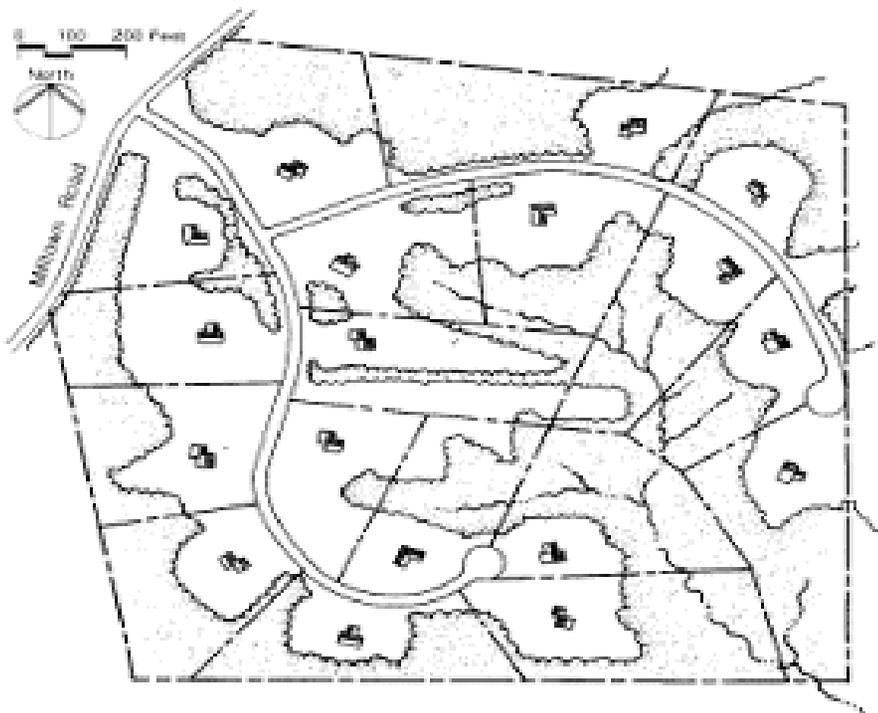


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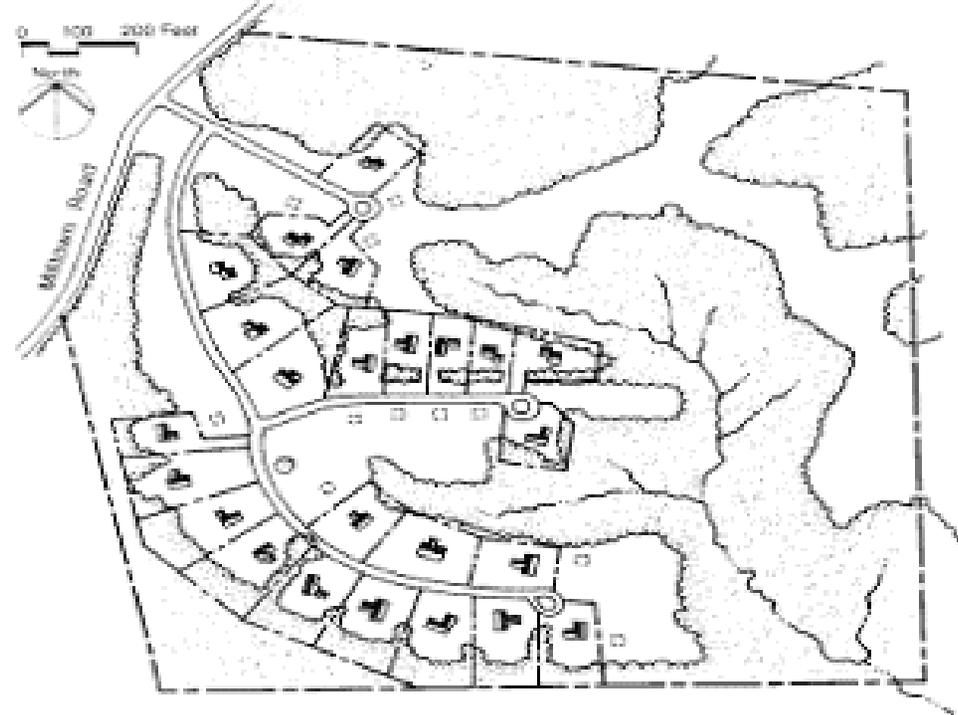


Source: Sumner County

REGULATORY



Conventional Subdivision

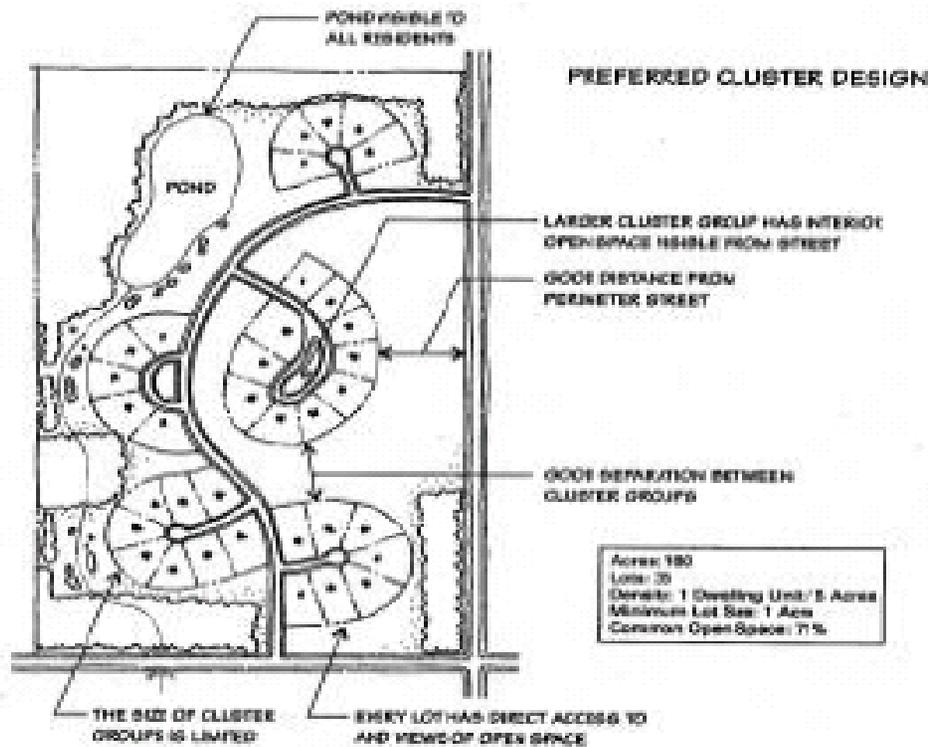
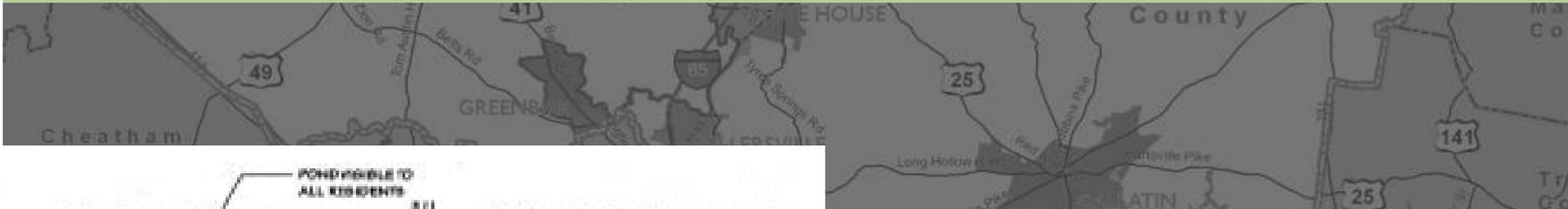


Open Space Subdivision

Source: Growing Greener: Conservation by Design from <http://www.natlands.org/planning/growgreen.html>

REGULATORY





Potential Benefits

According to the article, *Better Designs for Development*, Conservation development or subdivisions potentially can benefit a community in a variety of ways:

- Achieves a community goal of preserving open space at the same density standard as is outlined in current ordinances.
- Establishes an open space network, if done within the context of a comprehensive plan and these types of developments/subdivisions are purposefully linked together. Continuous open space (farmland, forest or other natural resources) allows for greater benefits for the environment, i.e., habitat preservation for wildlife, and for a local economy if dependent on agriculture and/or tourism. This open space network also can extend and join recreational trails.
- None of the land is taken for public use unless the developer/owners want it to be.
- Does not require public expenditure of funds.
- Does not involve complicated regulations for shifting rights to other parcels.
- Does not depend upon the cooperation of two or more adjoining landowners to make it work.
- Provides a quality residential and recreational environment.

Source: Mt. Juliet

Wilson MPO Planning Boundary (Portion Shown)

County Boundary

Water Body

State Highway



REGULATORY



5. To promote interconnected greenways and corridors throughout the community, and create contiguous greenspace within and adjacent to development sites.
6. To protect scenic views.
7. To protect prime agricultural land and preserve farming as an economic activity.

B. Applicability

A cluster residential subdivision is permitted in the R-R, R-T, R-SF, R-20 and R-14 districts subject to the following standards.

C. Subdivision Design Process

Cluster option subdivisions shall be designed around recreation and open space. The design process should "land-based", and commence with the delineation of all potential open space, after which potential building sites are located. Following that, road alignments are identified, with lot lines being drawn in as the final step. This "four-step" design process is further described below:

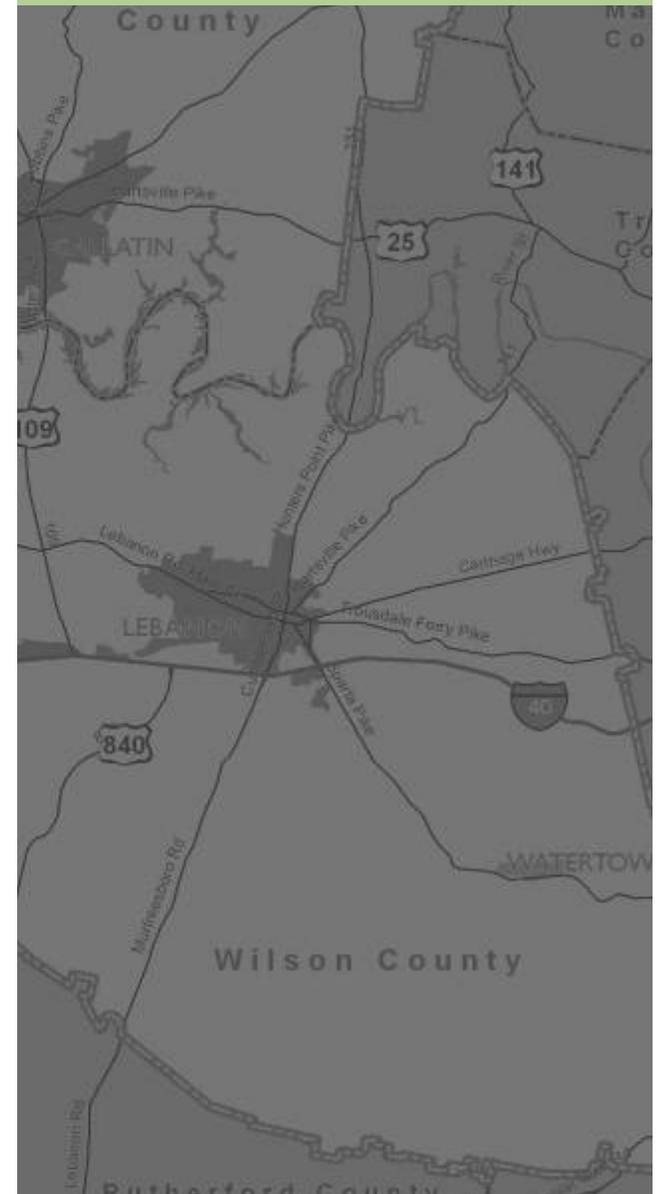


STEP 1: OPEN SPACE DESIGNATION

All potential conservation areas shall be identified using a Site Analysis Map. See §3.3.4 for using conservation areas to meet minimum open space requirements.



REGULATORY



Choices: Tools Available

PROGRAMS

- Conservation easements



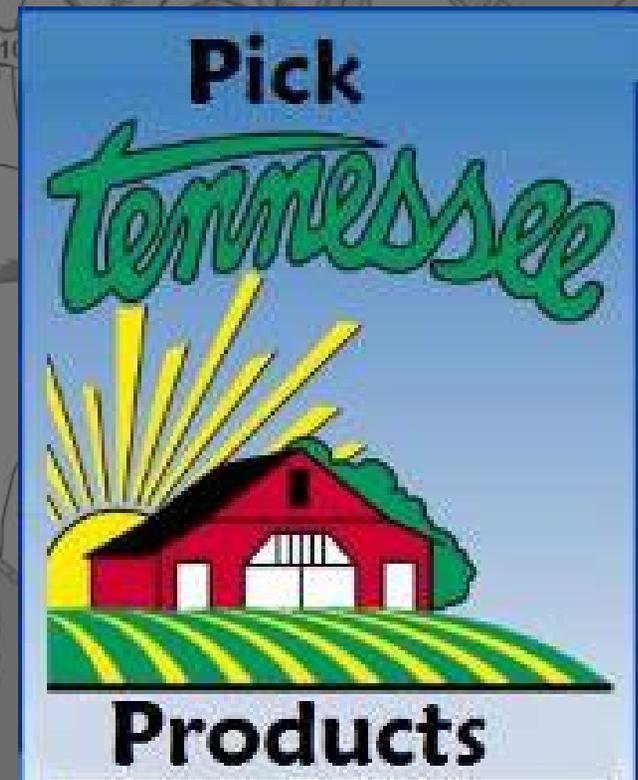
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state Highway



Choices: Tools Available

PROGRAMS

- Programs / assistance supporting agricultural, other types of businesses
 - Statewide Ag Producer Association Grant Program (TDA)
 - Farmers Market Grant Program (TDA)



Choices: Tools Available

PROGRAMS

- Agricultural districts / easements



Choices: Tools Available

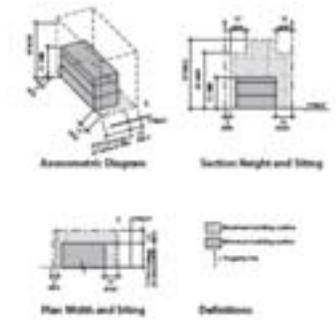
REGULATORY

- Form-based zoning (with flexible standards)

PROPERTY DEVELOPMENT **SW2**

4.2-5 BUILDING SITING & CONFIGURATION

NOTE: Provide building configuration and design parameters, as well as suggestions for building location.



PRINCIPAL BUILDING SITING:

Orientation:	NA
Front Setback:	10' Min
Frontage at Setback:	10% Min
Side Setback:	25' Min
Rear Setback:	5' Min
Lot Size:	1 Acre Min
Building Coverage:	75% Max
Open Space Coverage:	25% Min

PRINCIPAL BUILDING CONFIGURATION:

Building Height Min:	25' & 2 Story Min
Building Height Max:	40' & 3 Story Max plus 10' & 1 Story Max at Setback + 10'
Roofline / Floor Plate:	NA

FLOORAREA RATIO (FAR):

	1 Max
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438

Source: Knoxville

§4.2.1 Zero Lot Line

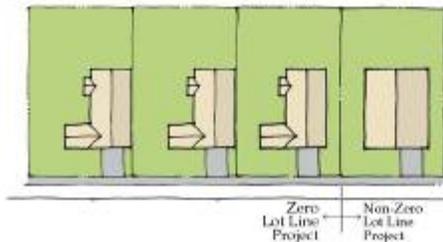
§4.2. Residential Use Standards

The following standards shall apply to all permitted uses, discretionary uses, and conditional uses, as set forth in the Permitted Land Use Table (see §2.2.2).

§4.2.1. Zero Lot Line

A. A single side yard shall be provided comprising the equivalent of two side yards of a conventional detached house. This reduction shall not be allowed on the road yard or to the side yard adjacent to lots that are not part of the zero lot line project.

B. An easement between the two property owners to allow for maintenance or repair of the house shall be required when the roof overhang or side wall of the house are within four feet of the adjacent property line (no roof overhang shall be permitted to extend across the property line). The easement on the adjacent property must provide at least five feet of unobstructed space. The easement shall be recorded on the subdivision plat.



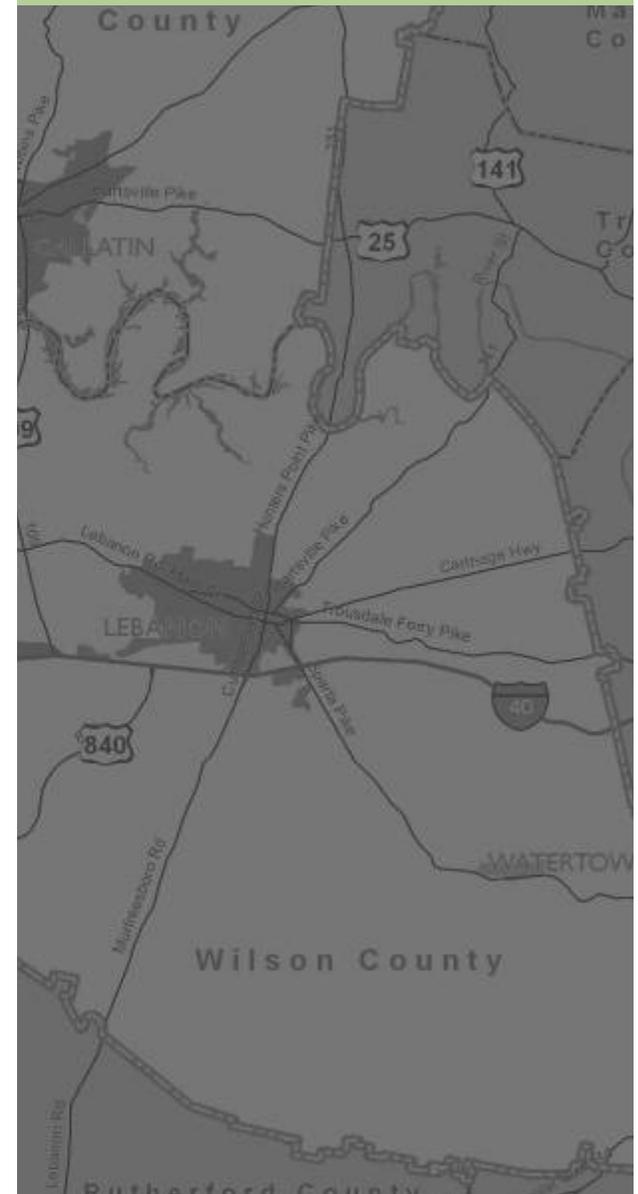
§4.2.2. Alley-Loaded House

An alley shall be provided to the rear of all alley-loaded houses. All vehicular access shall take place from the alley and all parking shall be in the rear yard. No parking shall be permitted in the required road yard.

§4.2.3. Two-family

- A. Access shall be provided from a single driveway or from an alley, where available; and no parking space shall be located in a required yard, except for the rear yard.
- B. The principal structure shall be designed and appear to be a single family detached. A single, shared front porch of at least 100 square feet shall be provided and front door access to each dwelling shall be from the shared porch.
- C. No off-road parking space shall be located closer than ten feet to any residential building wall.
- D. For developments of 40 or more dwelling units, a divided ingress-egress driveway with a landscaped median for the first 400 feet of all entrances from public roads shall be provided for all developments.

REGULATORY



Choices: Tools Available

REGULATORY

- Design standards

6-103.7 Design Standards for Commercial Structures (Amended by Ordinance 2005-35, 12/12/05)

- Site features such as landscaping, site fences, walls, refuse and recycling containers shall be coordinated into a unitary design and shall otherwise comply with all provisions of Article X, (Landscaping and Screening). All utility service to individual structures shall be underground.
- Building(s) may be required to incorporate similar design elements, and shall be compatible with surrounding buildings with regard to massing, color, scale, proportion of openings, roof types, types of glazed openings, and degree of detail. Color and design elements shall also be compatible with buildings in the vicinity which have been approved in accordance with these design provisions. On corner lots, both sides of the building facing the street are considered the front of the building, and shall include design elements such as windows, doors, texture and wall treatment to provide visual interest and prevent development of a long continuous blank wall.

Commercial and institutional buildings which are visible from a public street are required to have a minimum of seventy percent (70%) brick and stone on the façade(s) facing a public street and fifty percent (50%) brick and stone on all other façades. Commercial and institutional buildings which are not visible from a public street are required to have a minimum of fifty percent (50%) brick and stone on all façades. Where split face concrete block is to be used as a secondary material, it shall be used in combination with brick or stone and shall not exceed fifty percent (50%) of the area of each wall. Where EIFS or synthetic stucco is to be used as secondary material, it shall not exceed fifty percent (50%) of the area of each wall and shall be at least two feet above grade and above a brick or stone base. The use of vinyl siding, exposed or painted corrugated metal or plain sheet metal roofs, exposed or painted metal siding, and exposed standard concrete block are prohibited.

Building service areas or loading areas shall not be visible from public streets and shall be located away from private streets and adequately screened. Mechanical equipment on roof or sides of buildings shall not be visible from public streets and must be adequately screened.

Choices: Tools Available

REGULATORY

- Local historic district / guidelines

Downtown

Goal 3: *Enhance the historic characteristics of Lebanon's downtown district in order to create a distinct city center.*

Objectives:

- Utilize historic zoning overlays to ensure that new developments and revitalization efforts within historic districts maintain their integrity and uniqueness for future generations.
- Increase public awareness of downtown historic resources by providing signage along road ways to draw attention to historic sites and districts.
- Preserve and protect historically significant sites and buildings.
- Coordinate efforts with the Beautification Committee to enhance the historic elements of downtown.

Source: Lebanon

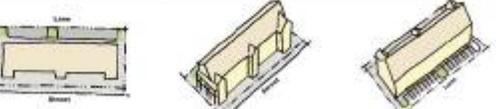
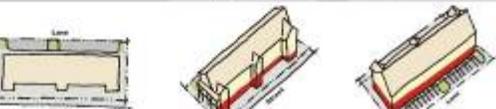
§2.2.1 Housing Types

§2.2. Permitted Land Uses

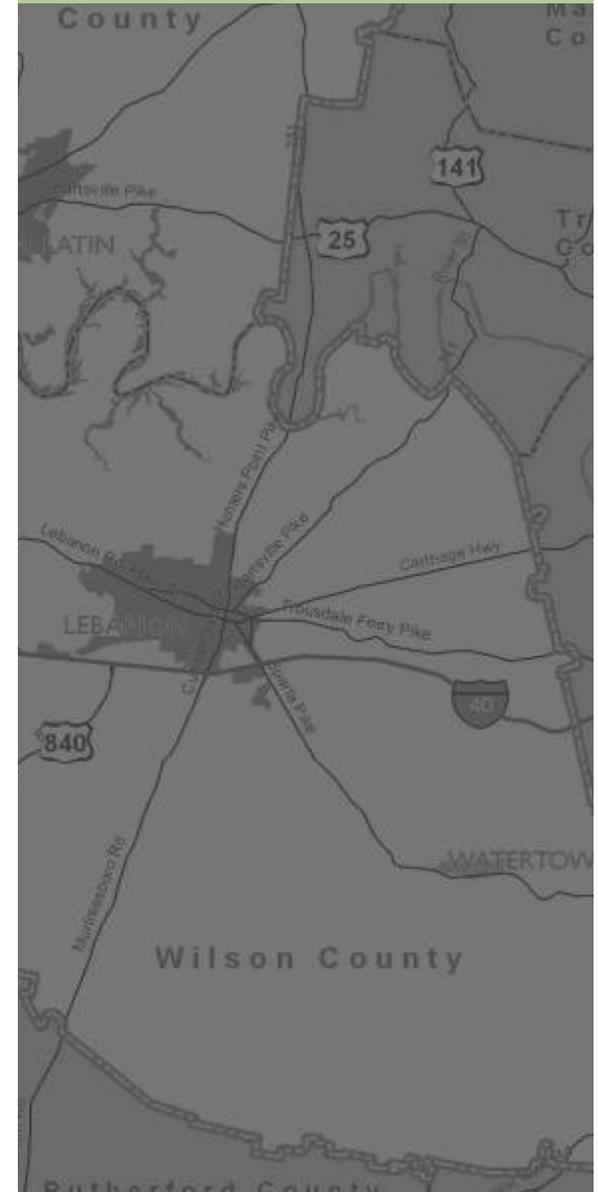
§2.2.1. Housing Types

A. Definitions

The following housing types are established to provide a common terminology for housing in Lincoln County. All drawings are for illustrative purposes only.

<p>SINGLE FAMILY DETACHED A detached dwelling unit located on a single lot with private yards on all four sides</p>	
<p>ZERO LOT LINE HOUSE A detached dwelling unit located on a single lot with private yards on three sides. The unit has a single side yard on one side comprising the equivalent of two side yards of a single family detached house.</p>	
<p>ALLEY-LOADED HOUSE A detached dwelling unit located on a single lot with private yards on all four sides; however, the house is set closer to the road than a single family detached house and parking is in the rear off an alley or lane.</p>	
<p>TWO-FAMILY HOUSE Two attached dwelling units in a single structure on a single lot (often called a duplex). The two units can be located on separate floors or side-by-side.</p>	
<p>TOWNHOUSE Two or more attached dwelling units located on separately owned lots or on a single lot where the units are lined up in a row and share side walls, individual units can be mixed vertically.</p>	
<p>MULTI-FAMILY Three or more attached dwelling units in a single structure on a single lot. A multi-family can vary in height from two to five stories; individual units can be mixed vertically.</p>	
<p>UPPER-STORY RESIDENTIAL A dwelling unit located on a floor above a nonresidential use.</p>	

REGULATORY



Choices: Tools Available

POLICY

SENIOR HOUSING

3.11. Encourage desirable housing for Seniors.

- 3.11. 1.** Create senior-friendly zoning and developer incentives that would make allowances in selected districts for senior communities.
- 3.11. 2.** Research the need for senior transportation throughout Hendersonville; possibly establish van service to and from doctor's offices.
- 3.11. 3.** Expand Senior Citizens Board to include non-senior members such as an attorney, a recreation department member, a doctor, and a developer.

AFFORDABLE HOUSING

3.12. Encourage Affordable Housing for Hendersonville's sons and daughters and workforce.

- 3.12. 1.** Identify code violations on single family properties and require repairs.
- 3.12. 2.** Create zoning and developer incentives for affordable housing.
- 3.12. 3.** Encourage reinvestment in specified neighborhoods through street, sidewalk, and other infrastructure improvements.
- 3.12. 4.** Pursue federal grants and work with the State of Tennessee to tap state and federal programs and funds. Also, work with local banks and non-profit housing agencies to provide assistance for affordable housing.

Source: Hendersonville Tomorrow

Choices: Tools Available

PROGRAMS

- Affordable housing programs (state and local)
 - Tennessee Housing Development Agency
 - Housing Authority
- Adaptive reuse
- Tax credits
- Partnerships
 - organizations, not-for-profit (Habitat for Humanity)



Tennessee Housing Development Agency was established in 1973 by the Tennessee General Assembly, TCA §13-23-101, et seq. The enabling legislation states our purpose:

To promote the production of more affordable new housing units for very low, low and moderate income individuals and families in the state,

To promote the preservation and rehabilitation of existing housing units for such persons, and

To bring greater stability to the residential construction industry and related industries so as to assure a steady flow of production of new housing units.

Strategic Priorities

- Preserve the supply of affordable housing
- Increase the supply of affordable housing
- Educate both providers and consumers of affordable housing
- Provide flexible products to meet market changes
- Create and cultivate a positive image of THDA and affordable housing

REGULATORY / INCENTIVES

- Density and Intensity bonus



REGULATORY / INCENTIVES

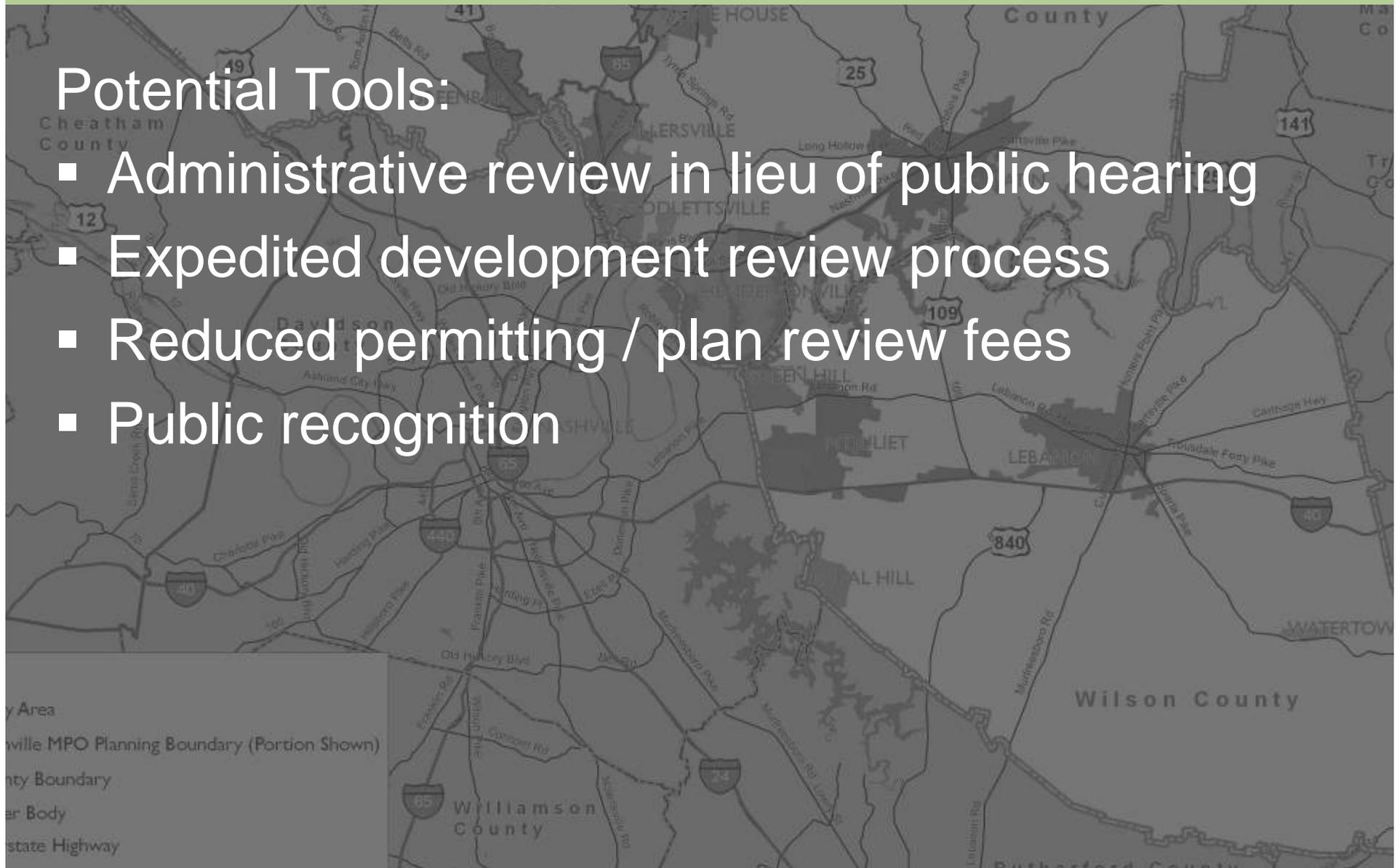


Choices: Tools Available

INCENTIVES

Potential Tools:

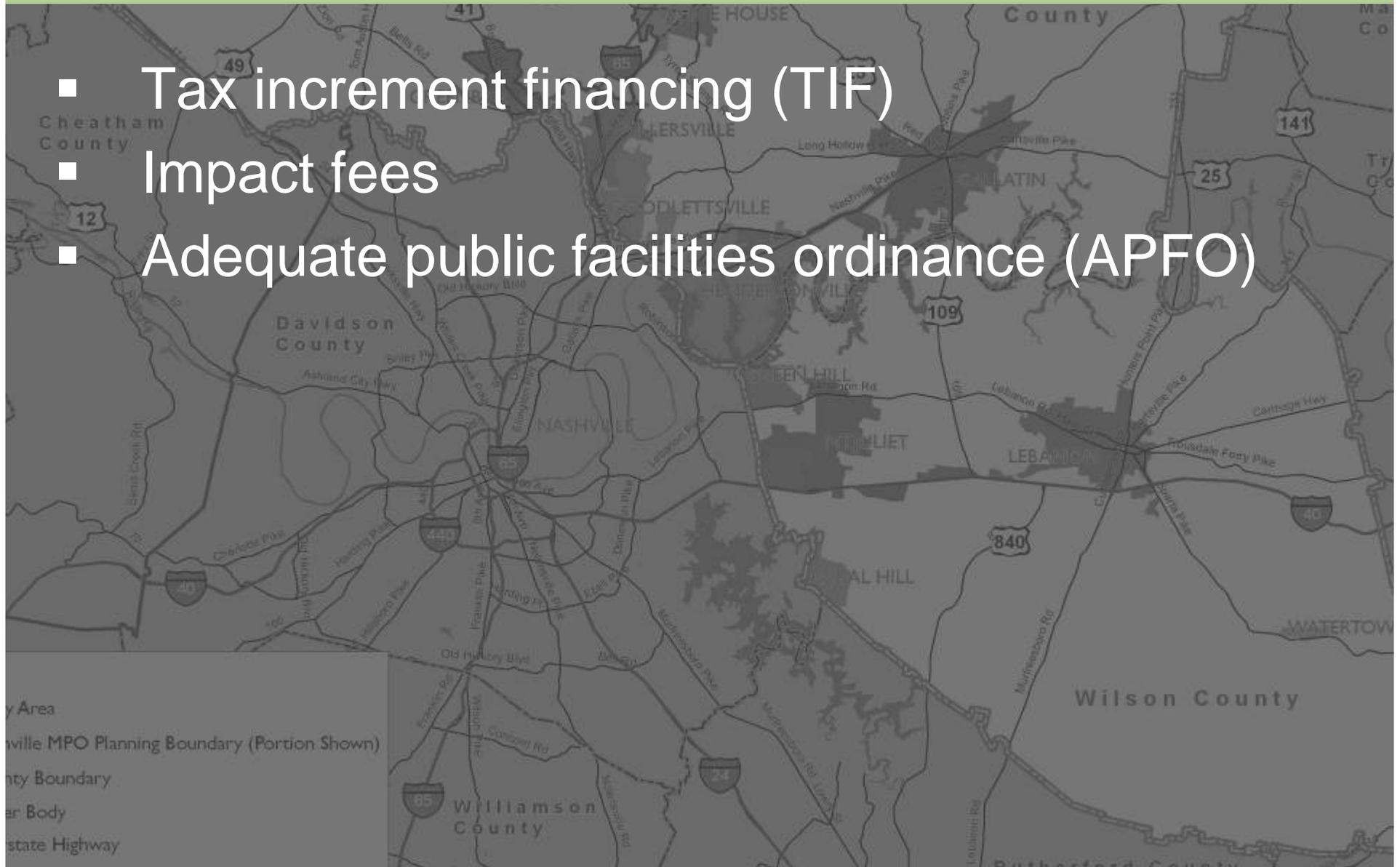
- Administrative review in lieu of public hearing
- Expedited development review process
- Reduced permitting / plan review fees
- Public recognition



Choices: Tools Available

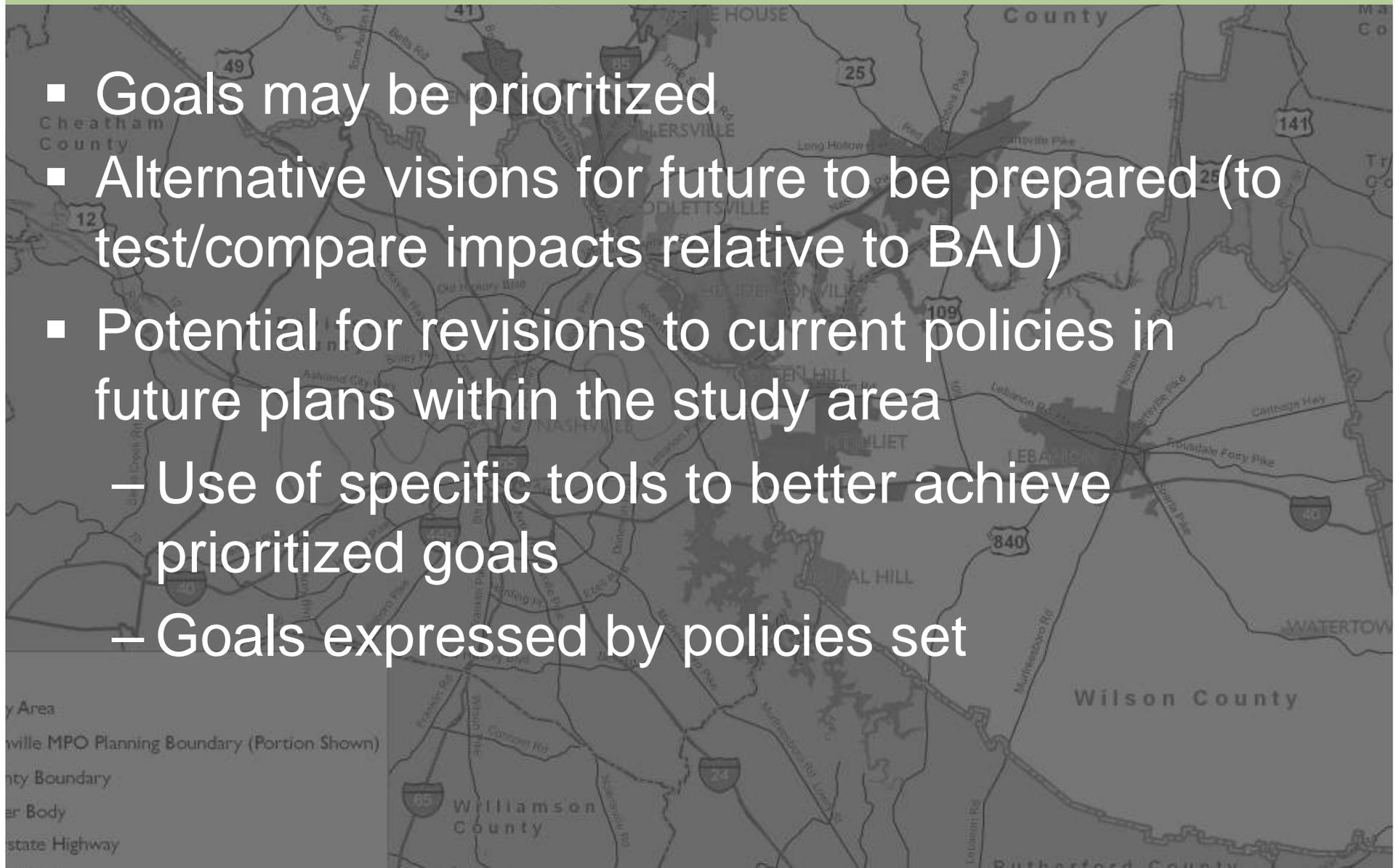
OTHER TOOLS

- Tax increment financing (TIF)
- Impact fees
- Adequate public facilities ordinance (APFO)



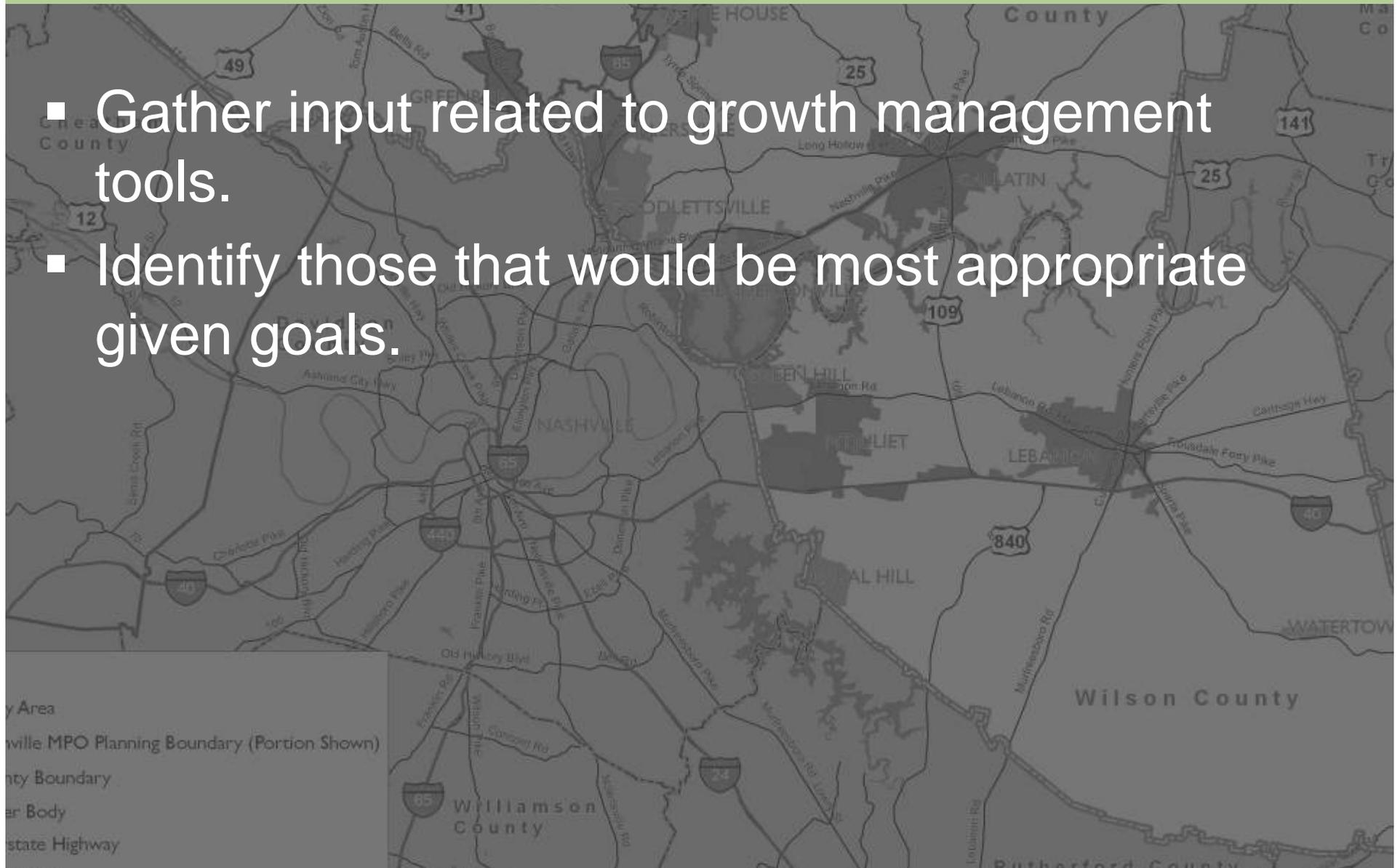
Considerations

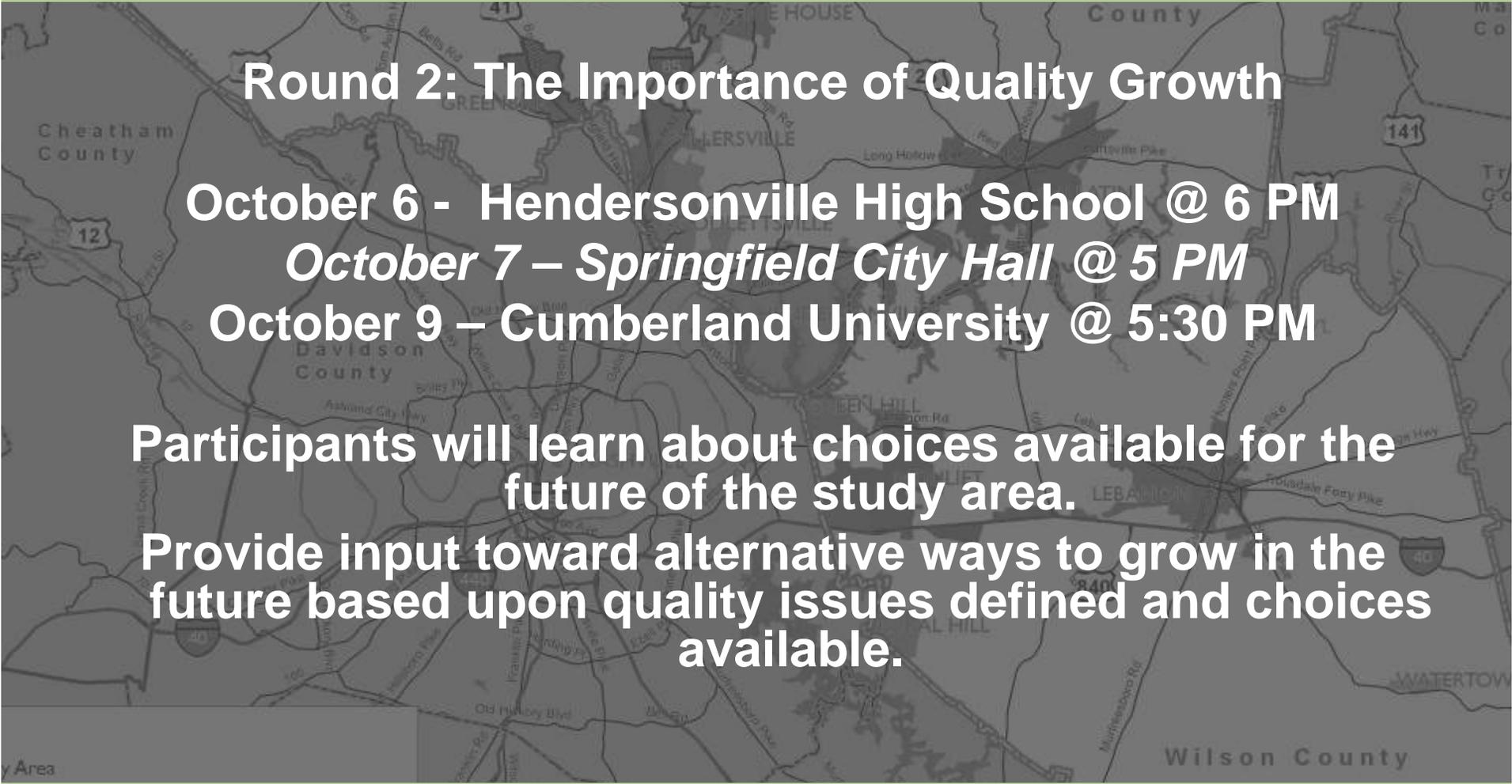
- Goals may be prioritized
- Alternative visions for future to be prepared (to test/compare impacts relative to BAU)
- Potential for revisions to current policies in future plans within the study area
 - Use of specific tools to better achieve prioritized goals
 - Goals expressed by policies set



Facilitated Group Discussion

- Gather input related to growth management tools.
- Identify those that would be most appropriate given goals.





Round 2: The Importance of Quality Growth

October 6 - Hendersonville High School @ 6 PM

October 7 – Springfield City Hall @ 5 PM

October 9 – Cumberland University @ 5:30 PM

Participants will learn about choices available for the future of the study area.

Provide input toward alternative ways to grow in the future based upon quality issues defined and choices available.